



**WEST
APPROACH:
DESIGN
STANDARDS**

DECEMBER 2021

DENREALESTATE.COM

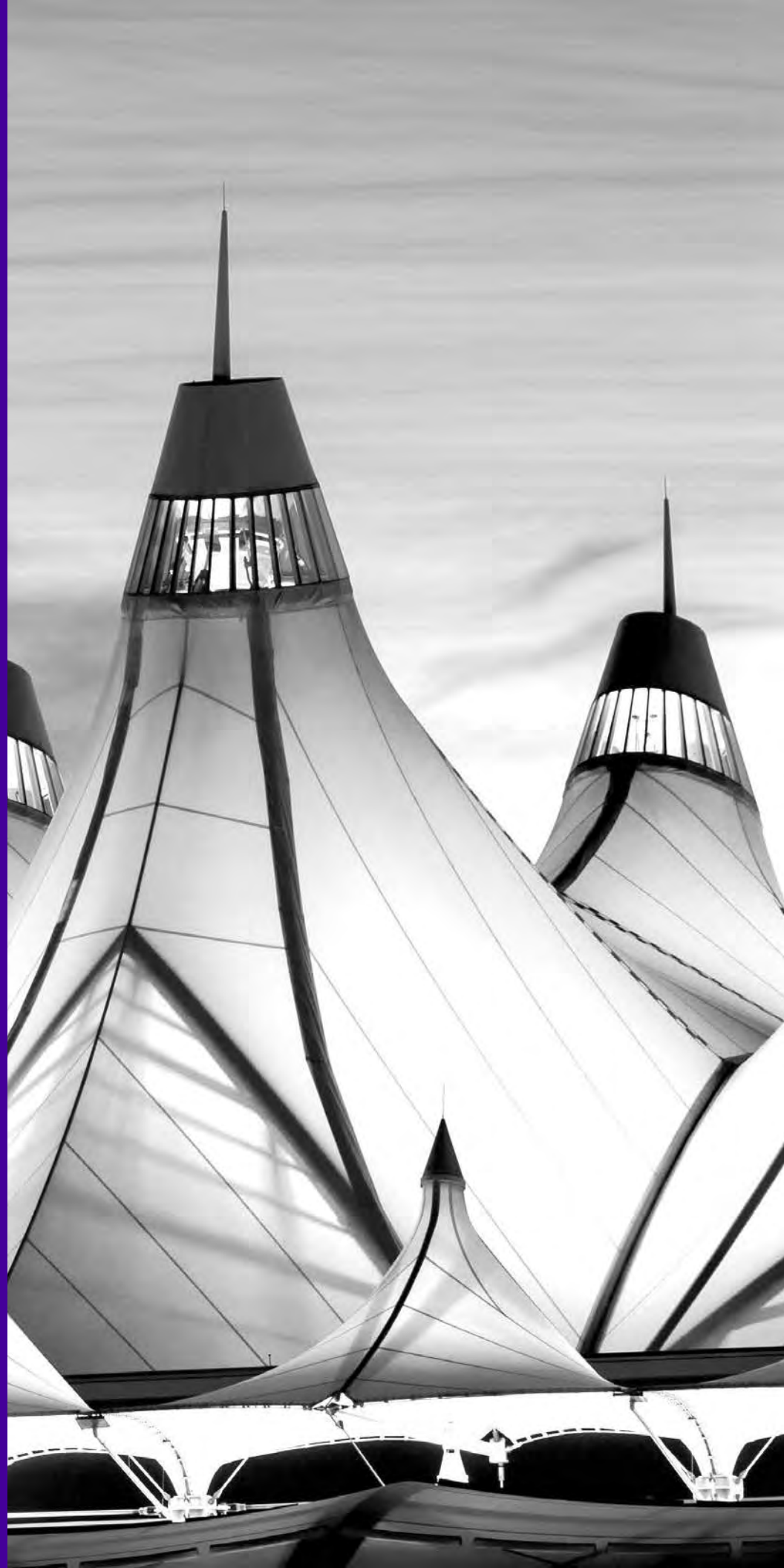


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OVERVIEW

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With approximately 53 square miles (13,700 hectares) of land area, Denver International Airport (DEN) has the second largest land ownership of any airport in the world. DEN's Division of Real Estate supports the airport's core aviation mission by capitalizing on DEN's size, capacity for growth, and centralized location, both nationally and globally, to create innovative, sustainable, and economically beneficial commercial development of the airport's non-aviation land. Commercial real estate development at DEN is positioned to leverage the region's rapidly growing economy, diverse industry clusters, expansive transportation network, and highly educated workforce.

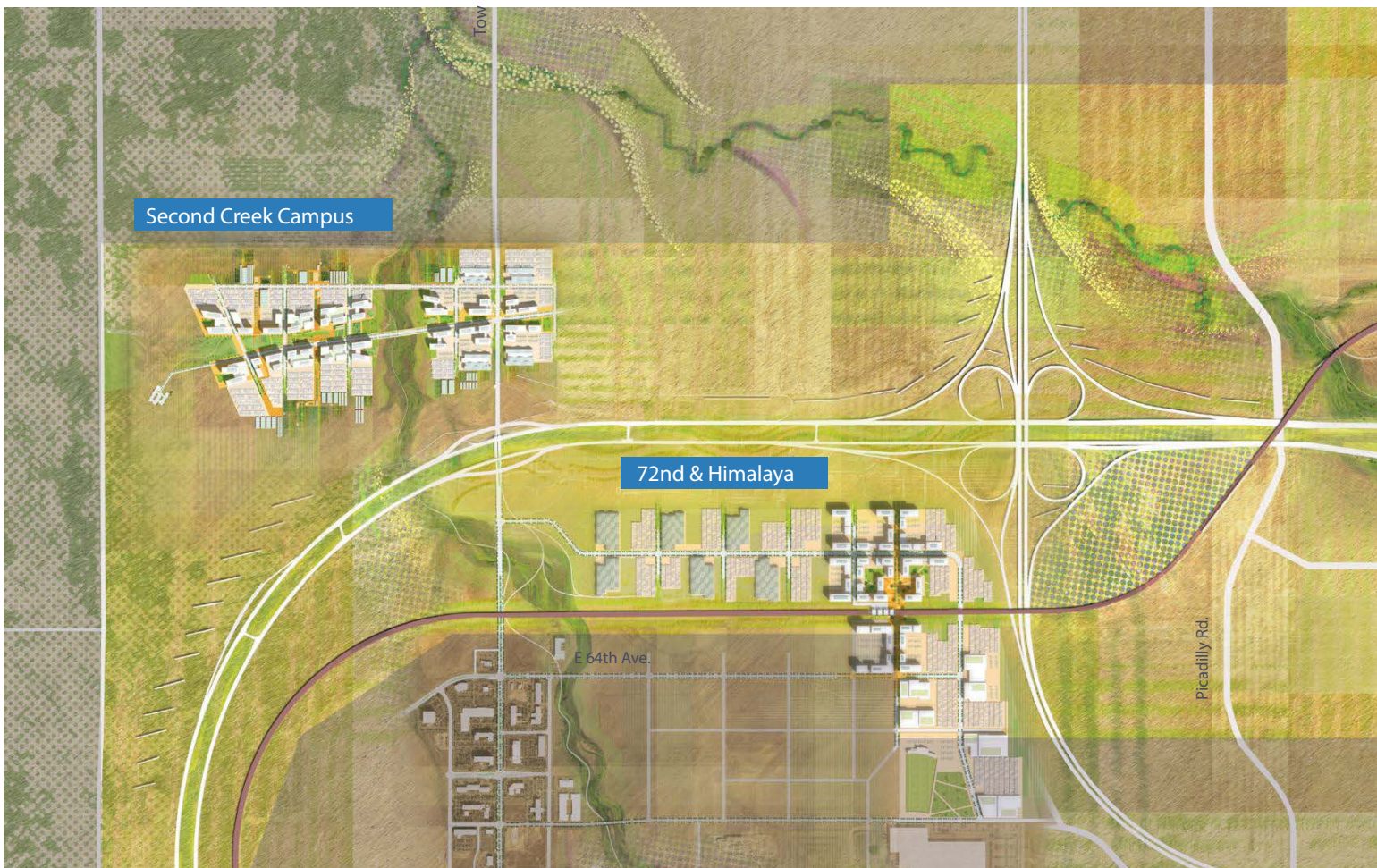
The Strategic Development Plan (SDP) guides this effort by creating a vision and roadmap for transforming airport land into a series of vibrant development districts that serve diverse local, national, and global businesses. The Design Standards work with and support the SDP by establishing a set of rules and regulations to guide design and implementation of development within each district. The Design Standards illustrate the overall design intent and how each District can achieve the design vision through the combination of some specific design requirements, or standards.



DEN STRATEGIC DEVELOPMENT PLAN & WEST APPROACH

STRATEGIC VISION

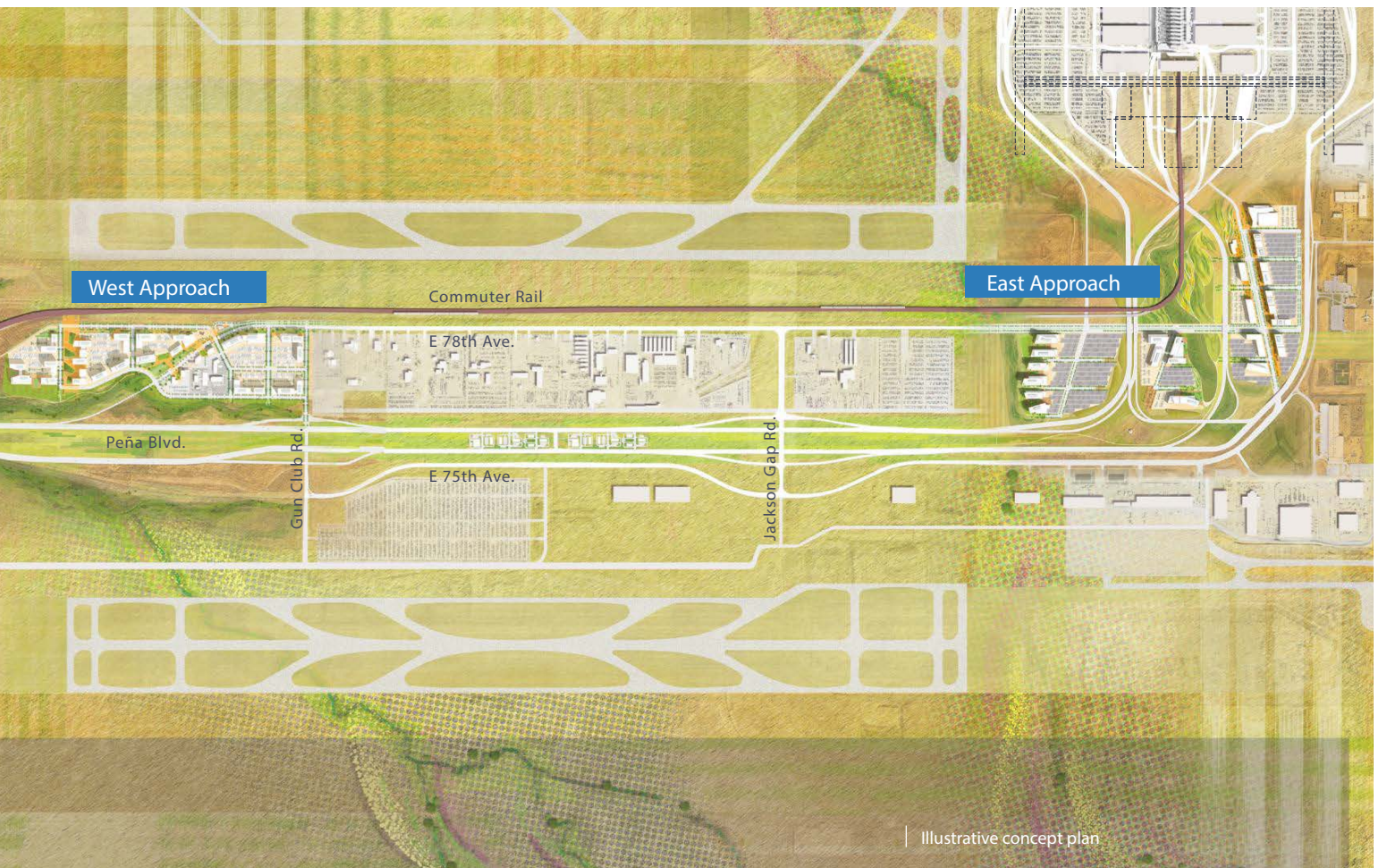
DEN's landscape context inspires the Strategic Development Plan. Within this landscape context, the plan creates distinct districts, each with a unique character and relationship to its surroundings. The districts are based on a framework of streets and public spaces designed to balance flexible growth and evolving technologies with the creation of attractive, human-scaled places. DEN's development districts will become destinations.



WEST APPROACH VISION & OVERVIEW

West Approach is an important gateway to DEN. The district is organized with buildings along Peña Boulevard, 78th Avenue, and the RTD A Line commuter rail. This orientation reinforces the district's street-fronted urban character and maximizes visibility for hotels, restaurants, offices and others seeking prominent locations along the Peña Boulevard corridor. In addition to exceptional access to the region's highway system and DEN's rental car services, West Approach links to Jeppesen Terminal via the proposed circulator transit system.

The district features three primary clusters of development. Unique outdoor spaces frame views of the dynamic operations of the airport, the natural prairie landscape, and the Rocky Mountains. The north-south orientation of streets and public spaces highlights aviation vistas, including a signature plane spotting landscape aligned to a future runway.



Illustrative concept plan

PEÑA CORE/COMMERCIAL DEVELOPMENT DISTRICTS AT DEN

Peña Boulevard is the gateway to DEN and the greater Denver metropolitan area. It shapes the experience of airport users and visitors, and serves as a major transportation corridor for development occurring in the northeast quadrant of Denver. The Peña Boulevard Experience: Guiding Principles apply to all DEN development districts, including the Second Creek Campus, 72nd and Himalaya, West Approach, and East Approach.



Six guiding principles, described in greater detail on the following page, support implementation of the DEN's vision for the Peña Boulevard corridor:

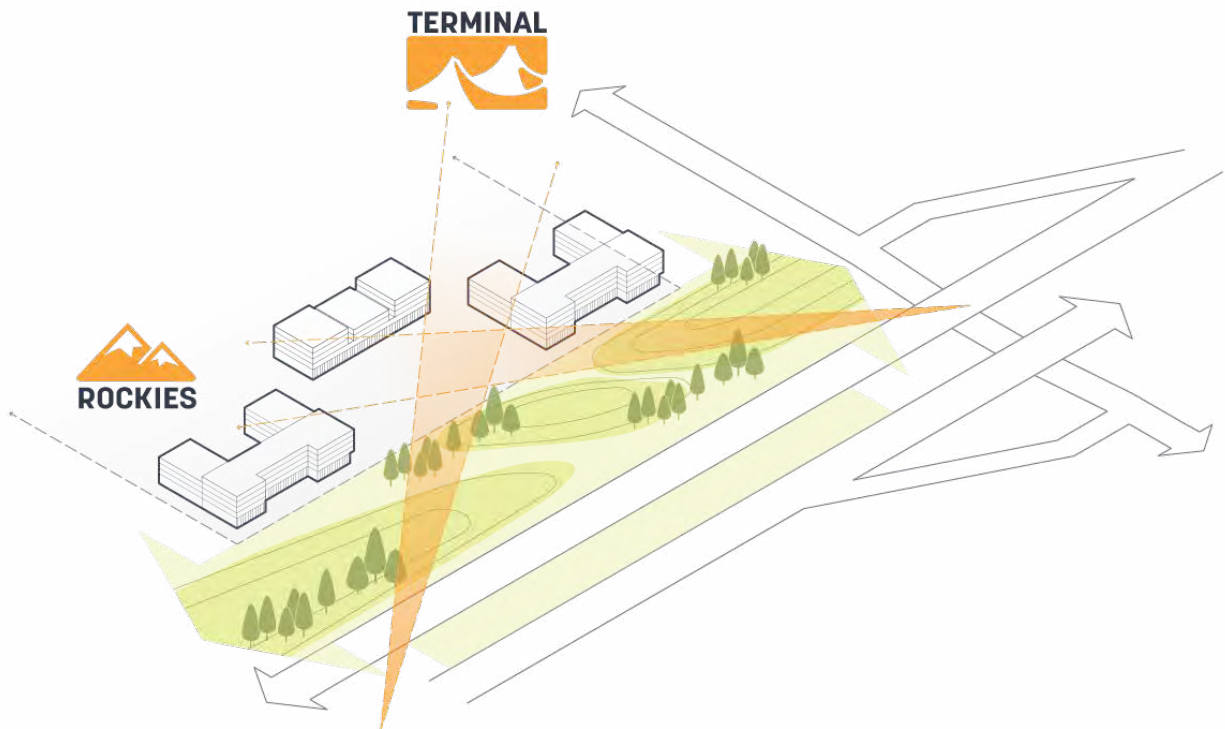
- » **Frame View Corridors to the Mountains and Terminal**
- » **Screen Visually Undesirable Uses**
- » **Create Prominent Architectural Presence**
- » **Provide Legibility of Access to District Entrances**
- » **Ensure Continuity with the Peña Experience**
- » **Build a Compact, Cohesive District**

The principles are intended to provide an overarching framework and context for the specific standards that will guide West Approach site development, consistent with DEN's Strategic Development Plan vision.



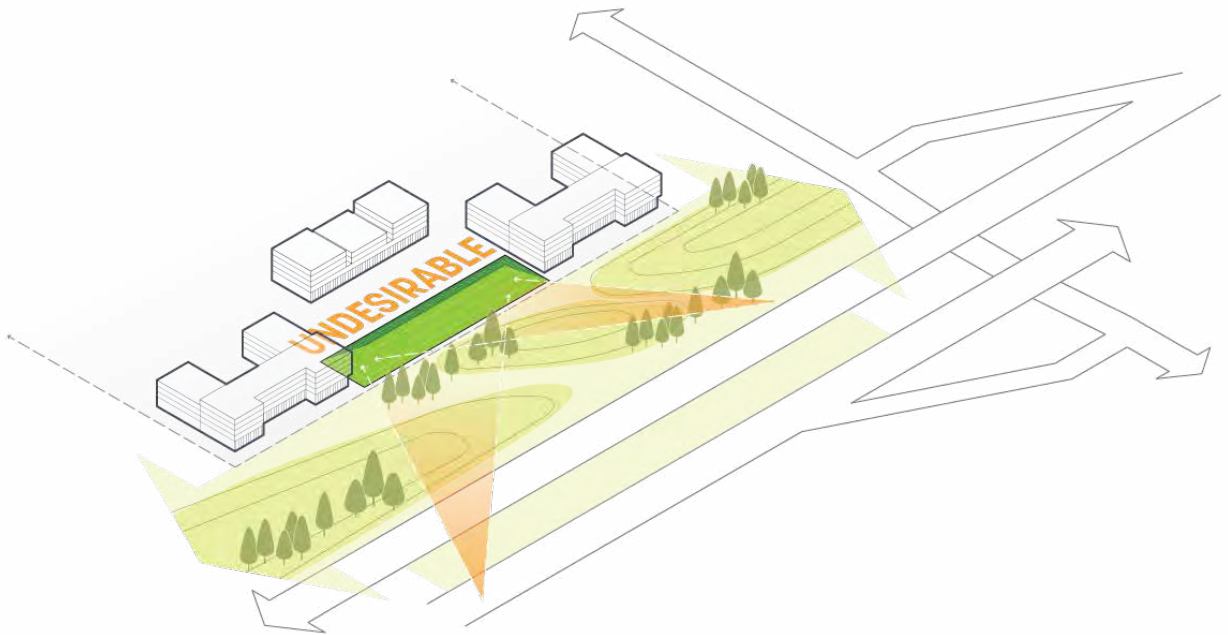
FRAME VIEW CORRIDORS TO THE MOUNTAINS AND TERMINAL

Buildings should be in an east-west orientation or kept low on the north-south axis to ensure that views to the Jeppesen Terminal and the Front Range are framed when appropriate, and minimally obstructed when required. Development should utilize building setbacks, massing, siting, orientation, scale, and other approaches to enhance views from Peña Boulevard and the commuter rail to the terminal and mountains. These strategies should be the basis for evaluating each individual buildings as well as overall district massing. Measurable evidence should be used to demonstrate the effectiveness of building siting and design related to view enhancement and impacts.



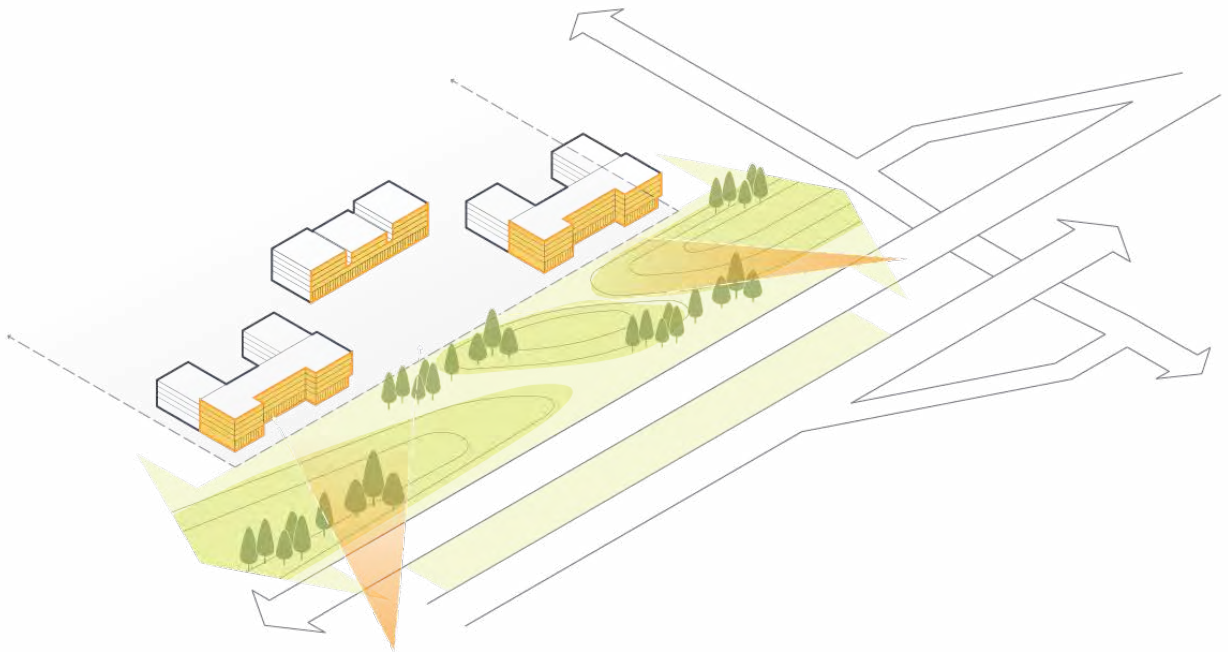
SCREEN VISUALLY UNDESIRABLE USES

Site and building design should screen visually undesirable uses from view from Peña Boulevard and the commuter rail. These uses could include parking, fuel pumping stations, service areas, or drive-through lanes. Screening can include placing buildings along view corridors or, when needed, utilizing landscape elements such as berms. Any landscape screening should relate to the character of the surrounding Peña Boulevard corridor landscape and be consistent with DEN's broader planning and landscape strategy. Measurable evidence should be used to demonstrate the effectiveness of landscape elements in screening.



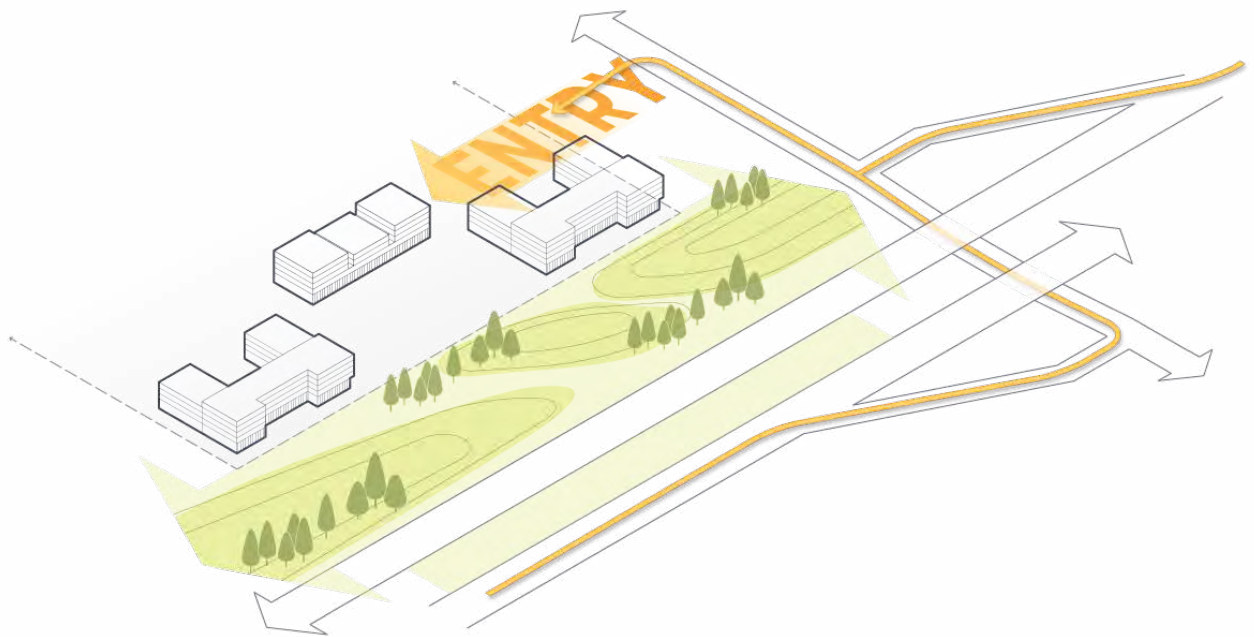
CREATE PROMINENT ARCHITECTURAL PRESENCE

Buildings should front Peña Boulevard and the commuter rail, with a distinct architectural character that contributes to the corridor-wide experience of the airport and prairie landscape. Building massing and landscape design should be harmonious, forming an overall impression that contributes to the visual qualities of the Peña Boulevard experience. Site design should avoid placing service uses, blank walls, parking garages, and other visually undesirable uses along district edges.



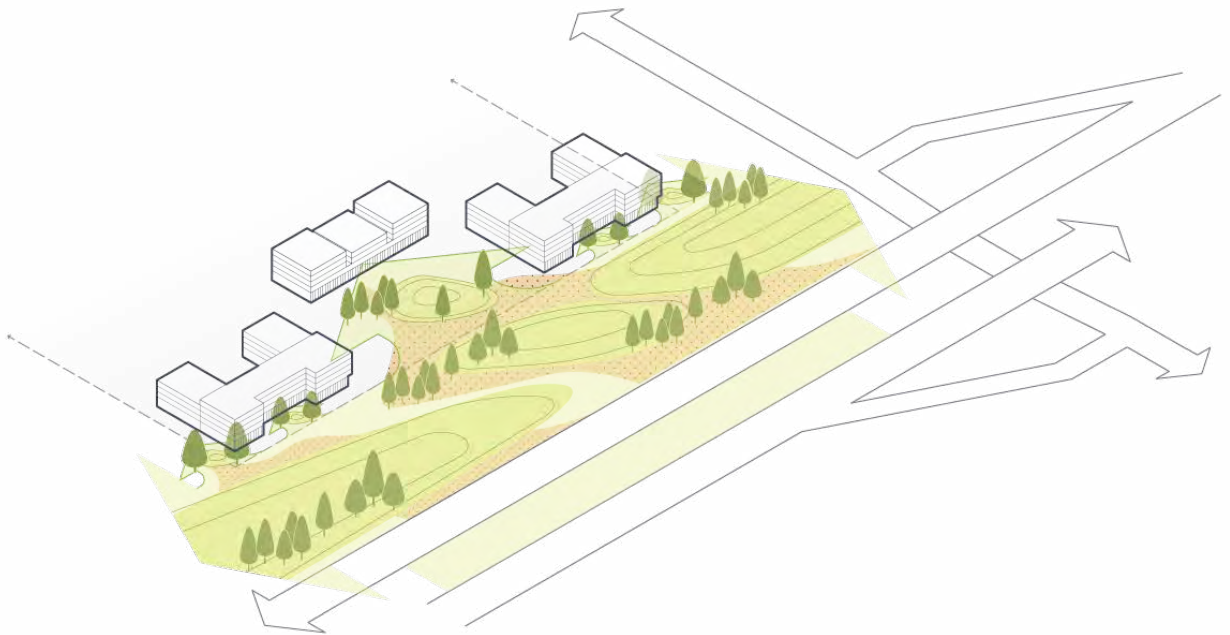
PROVIDE LEGIBILITY OF ACCESS TO DISTRICT ENTRANCES

District site design should support intuitive and direct access that is visually legible and functionally accessible from Peña Boulevard. The primary vehicular route should lead to major program elements within the district. Designs should utilize architectural massing, signage, and landscape to signify the primary access.



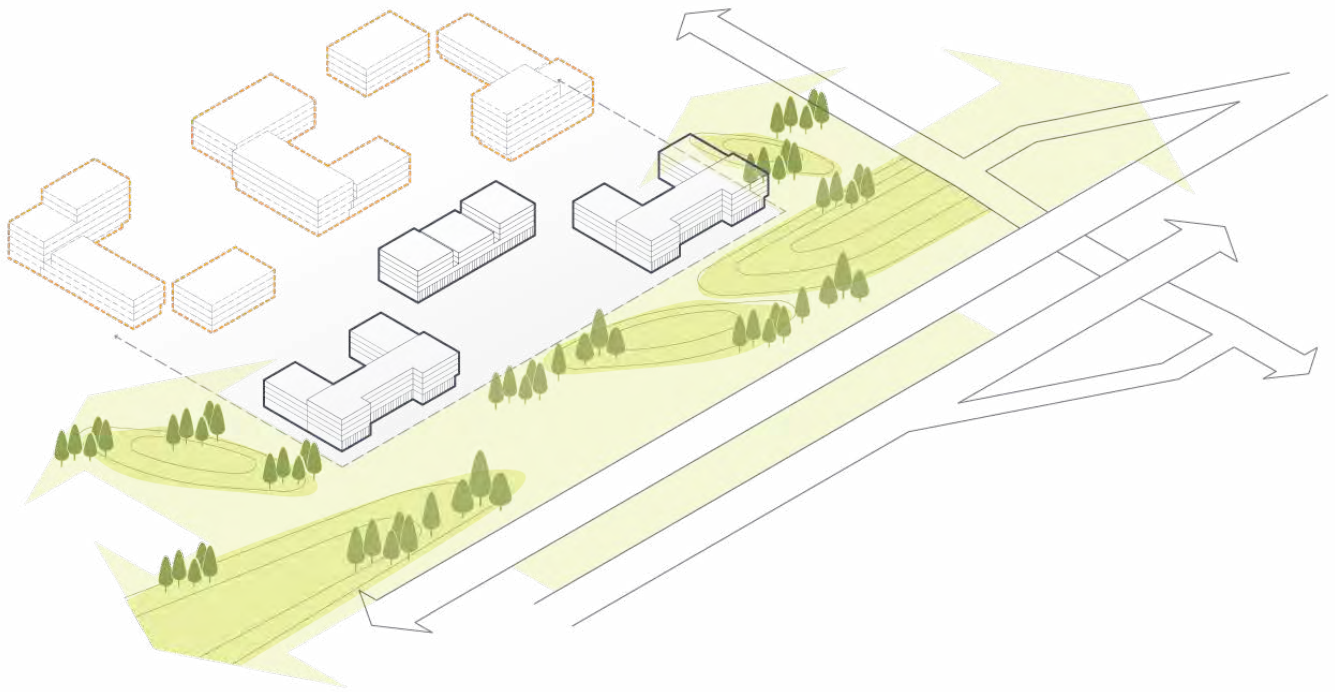
ENSURE CONTINUITY WITH THE PEÑA EXPERIENCE

District landscape design should contribute to and complement the overall Peña Boulevard experience as outlined in the Strategic Development Plan. Designs should intentionally blur the boundaries between the expansive, naturalistic corridor-wide landscape and district landscape by seamlessly continuing planting, land form, and drainage strategies.



BUILD A COMPACT, COHESIVE DISTRICT

Building design should contribute to a cohesive district identity by maintaining a complementary height, material palette, and architectural style. Site design should locate buildings/projects adjacent to each other to limit gaps in development. Each phase of development should cumulatively contribute to district placemaking. Buildings on district edges should reinforce district identity and define a distinct character within the corridor-wide experience.



ZONING

The West Approach is zoned DIA under Section 9.5 of the Denver Zoning Code. DEN will require that all respondents' development plans conform to the design standards described within this document.

The Denver International Airport encompasses 53 square miles of land. This large land area is designed to limit the impacts of airport activity on the airport's neighbors and provides room for the airport to expand and grow. The enormous potential of DEN to serve a prominent role in the global aviation system is due primarily to the airport's room to grow. With this ability to grow in a city that is otherwise landlocked, development within the DIA zone district provides a path to markets around the world and providing a stimulus for economic growth in the region.

ORGANIZATION OF DESIGN GUIDELINES & STANDARDS

INTENT

Intent statements establish more specific design goals or objectives for each sub-category topic addressed within the subject categories.

STANDARDS

Design Standards are prescriptive criteria that provide a specific set of directions for achieving the Intent Statements. Standards denote issues that are considered essential, and variations from the Standards must be specifically reviewed and approved by DEN. Standards use the term "shall" to indicate that compliance is required unless it can be demonstrated that an acceptable alternative meets one or more of the following conditions:

- An alternative method better achieves the stated Intent;
- The Intent will not be achieved by application of the Standard in this circumstance;
- The effect of other Standards or Guidelines will be improved by not applying this Standard.

DISTRICT LAYOUT

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The district layout at West Approach establishes a framework of streets, buildings and open spaces with the goal of creating an interconnected commercial and hospitality district that provides new amenities for passengers, employees and visitors at DEN. The block pattern is situated along Peña Boulevard, 78th Avenue, and the RTD A Line commuter rail to increase opportunities for views of the airfield and Front Range, and to maximize district visibility from Peña Boulevard. Street layout should balance the need for convenience and efficient vehicular mobility while also providing comfortable pedestrian connection between destinations and creating an inviting, character rich and easily accessible central open space. District layout should utilize building forms, block configuration, and location of open space to leverage the unique opportunities and establish the parameters that will guide a distinctive character throughout the West Approach District at DEN.



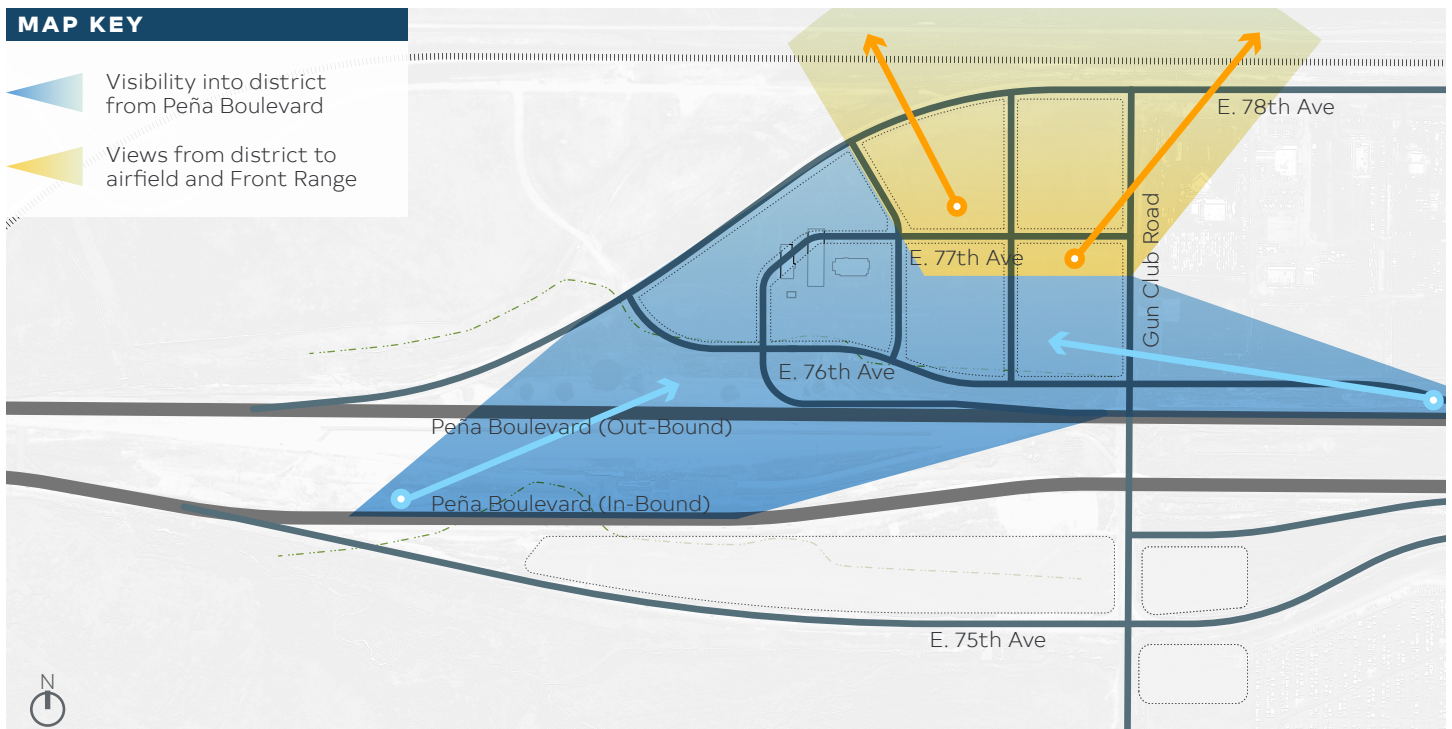
VIEWS

INTENT

- » Promote aviation character through visibility of DEN's airfield from streets and open spaces within the District.
- » Provide clear views to the development from Peña Boulevard.
- » Create a visual sense of arrival to the District from Peña Boulevard.
- » Preserve wide vistas of the Rocky Mountains and open prairie from outbound Peña Boulevard.
- » Comply with City and County of Denver protected view corridors that allow travelers approaching DEN on Peña Boulevard to see the iconic architecture of Jeppesen Terminal.

STANDARDS

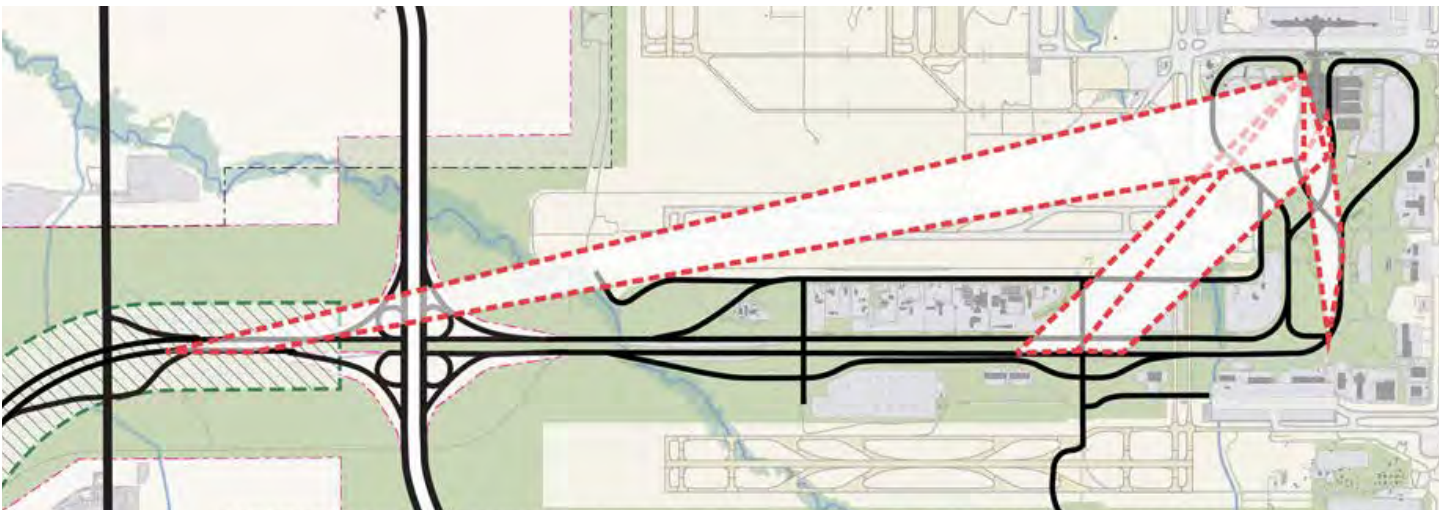
- » All buildings shall comply with City and County of Denver required view corridors.
- » Building placement and massing shall frame desired views.
- » Orientation of open space and amenities within the open space shall frame desired views.



General View Areas Along Peña Boulevard



City and County of Denver's Protected View Corridors to the Jeppesen Terminal



BUILDING HEIGHTS

INTENT

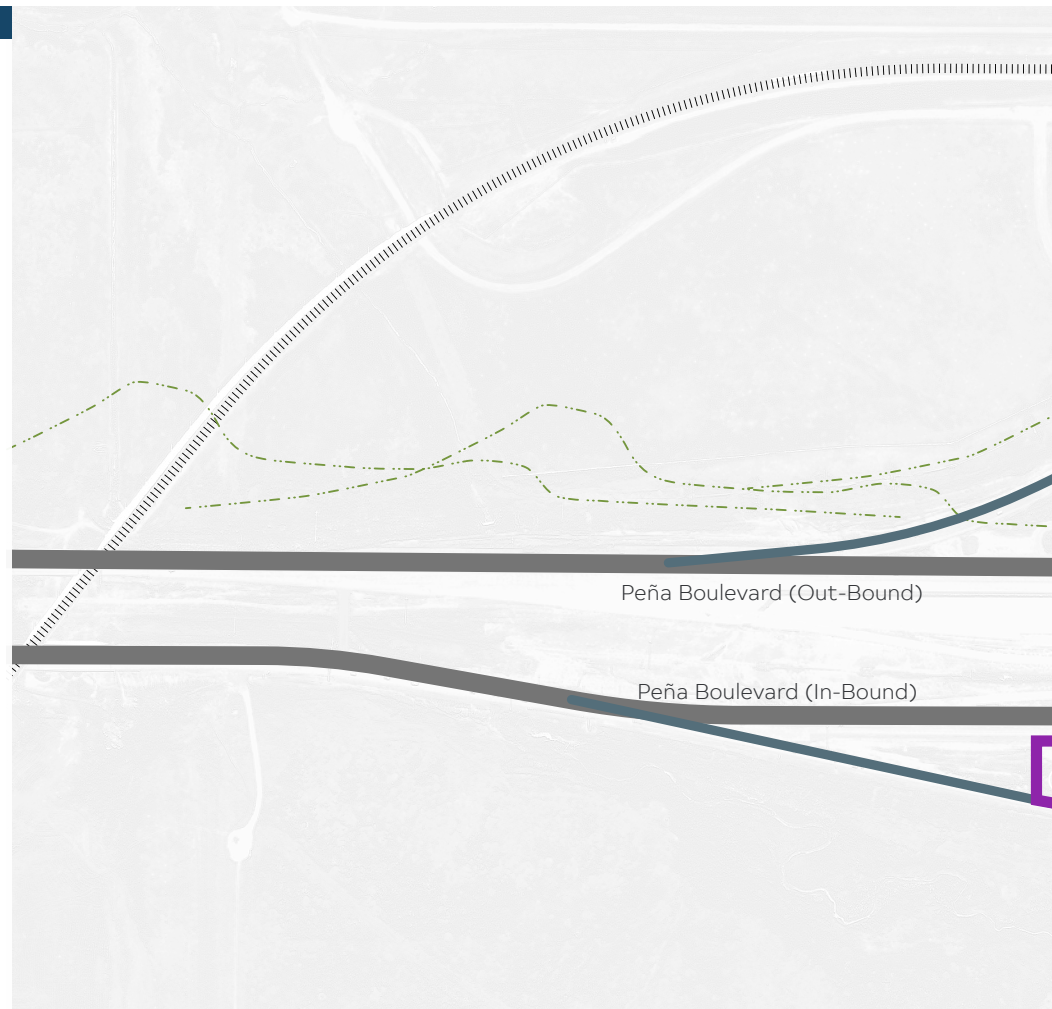
- » Maintain sight-lines from DEN's air traffic control tower per the FAA defined specific height regulations that are applicable to West Approach and all DEN property.
- » Stagger building heights relative to one another in order to leverage visibility into the District from Peña Boulevard and to maximize views of the airfield and Front Range from buildings and open spaces throughout the District.

MAP KEY

- 90-99 ft AGL
- 80-89 ft AGL
- 70-79 ft AGL
- 60-69 ft AGL

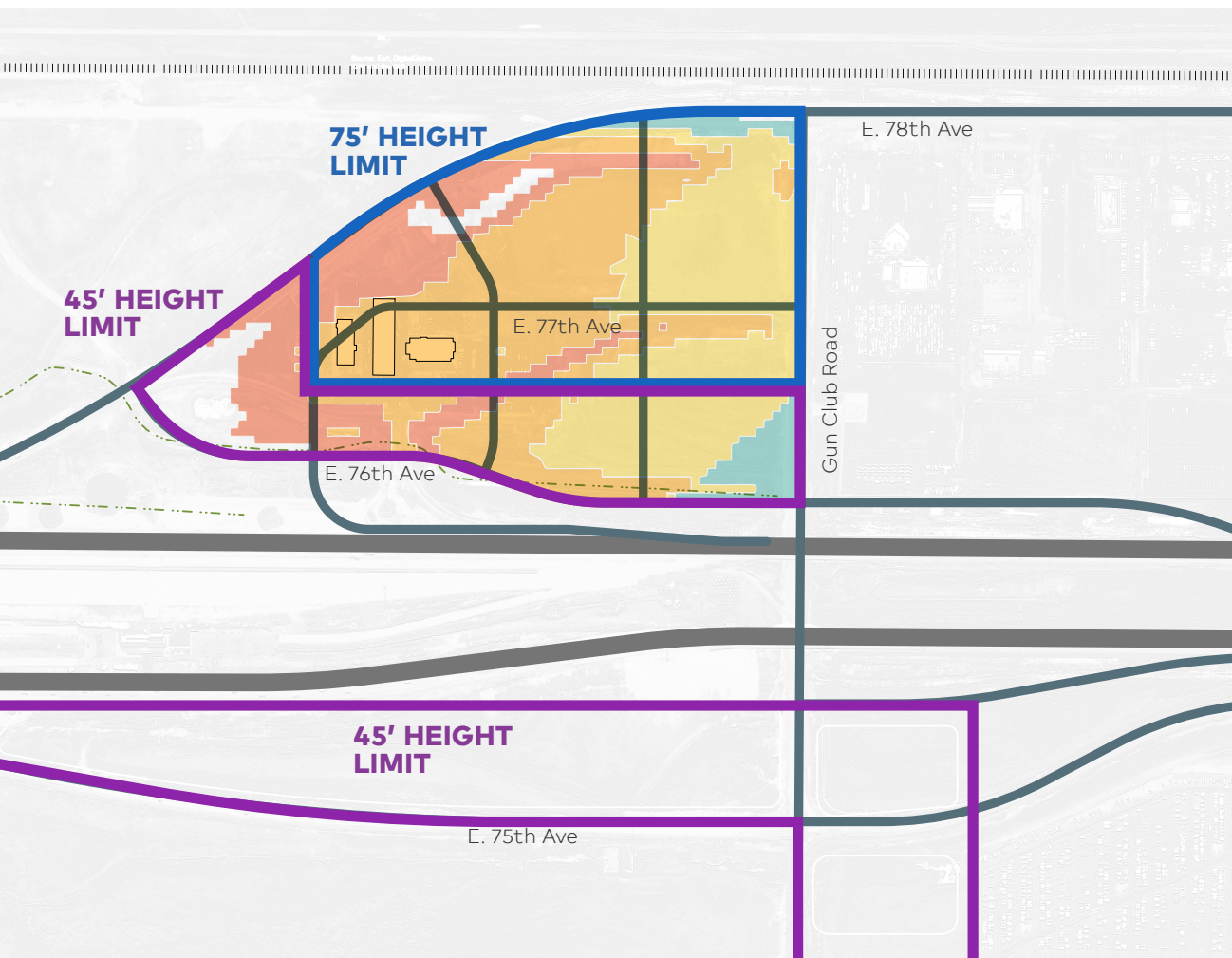
- 75' Maximum Building Height
- 45' Maximum Building Height

AGL (Above Ground Level):
FAA restriction on structure height above ground level, based on measurement from existing grade elevation



STANDARDS

- » No part of the building, building equipment or protrusion from a building shall exceed height restrictions set by the FAA. Site plan must be submitted to FAA for final approval of proposed building heights. FAA "Above Ground Level" height restrictions are based on existing ground elevation and provided as a reference but clearance is measured by the FAA in absolute elevation rather than a distance from the ground.
- » Maximum building heights shall be set in two zones with a lower height zone adjacent to Peña Boulevard per diagram below.



BLOCK CONFIGURATION

INTENT

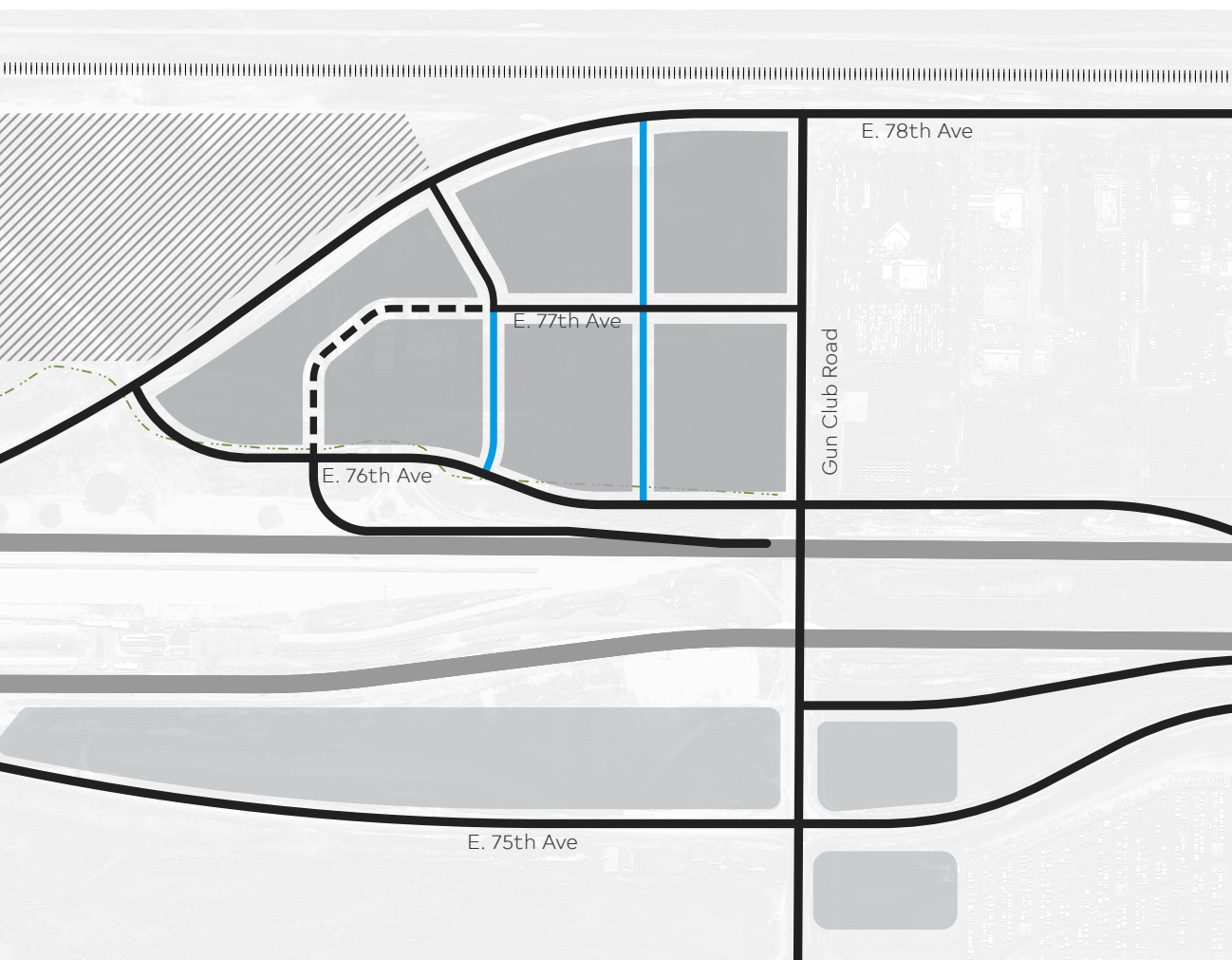
- » Organize and locate uses to complement adjacent uses or conditions.
- » Locate a central open space where it complements adjacent active uses.
- » Maximize ease of access to convenience based uses.
- » Streamline access to terminal from hospitality based uses.
- » Provide a street grid that is easily navigated by vehicles and pedestrians.
- » Retain operational and vehicular efficiency of airport serving streets on the edges of the District, including 78th Avenue, Gun Club Road and 76th Avenue.
- » Prioritize and support airport focused vehicular service on 78th Avenue and Gun Club Road.

MAP KEY

- Development Blocks
- Flexible Road
- Fixed Road
- - Future Road Condition



- » Provide continuous movement and efficient circulation within West Approach District.
- » Accommodate phasing and development needs with flexibility in final location of select interior streets.
- » Balance traffic needs with pedestrian safety and comfort.
- » Provide adequate street width for emergency services and maintenance operations.
- » Provide flexibility on exact location and alignment for north-south streets through the District.
- » Accommodate future road location near Final Approach when land use changes.



AIRPORT TRAFFIC PATTERNS

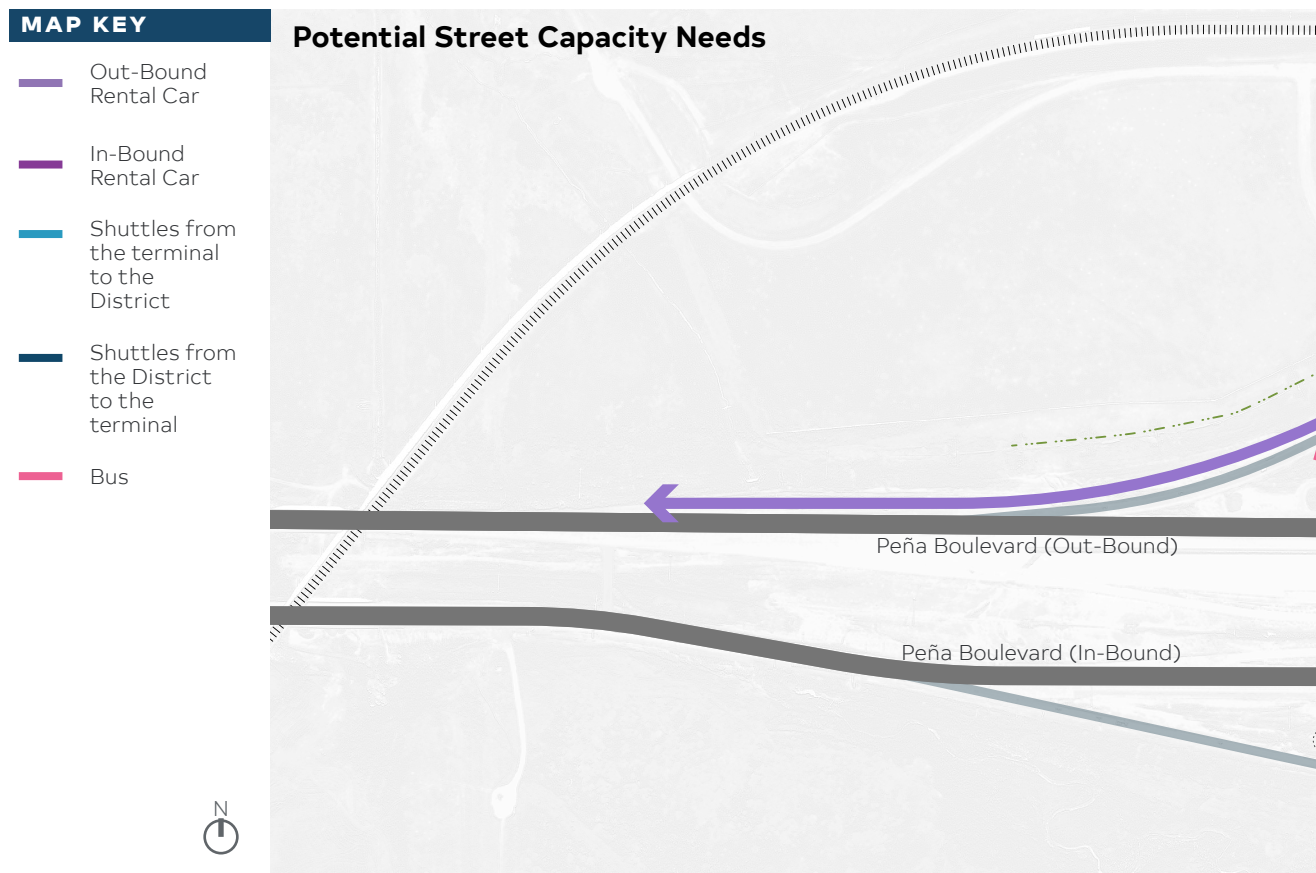
INTENT

- » Prioritize efficient vehicular movements for rental cars.
- » Provide efficient bus route along 76th Avenue.
- » Consider shuttle movements and stops throughout the District as development occurs.
- » Coordinate shuttle circulation routes to the District from the terminal via direct connect routes to 78th Avenue.
- » Provide shuttle circulation routes from the District to the terminal via the ramp from Gun Club Road to in-bound Peña Boulevard.

STANDARDS

Collector: 78th Avenue, Gun Club Road

- » Expedited return and expedited exits shall be provided for rental cars. These routes shall be accommodated along Gun Club Road and 78th Avenue, on the edges of the District.

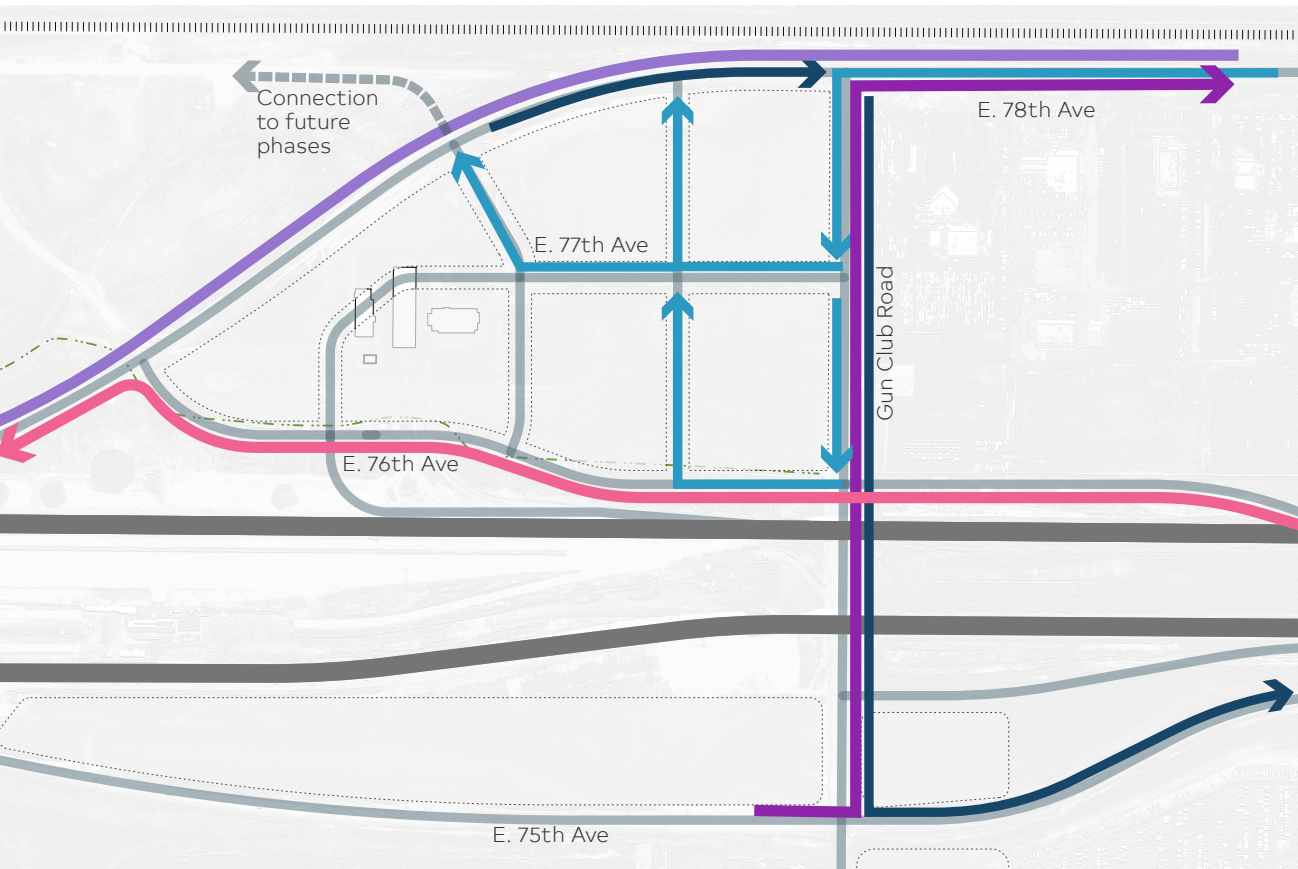


Partial collector: 76th Avenue

- » A bus route shall be accommodated along 76th Avenue at the southern edge of the District.
- » A maximum of 1 bus pull-out shall be provided at a minimum of 40' from the intersection.
- » Quick service retail vehicular access shall be accommodated along 76th Avenue near the commercial serving uses.

Interior Streets

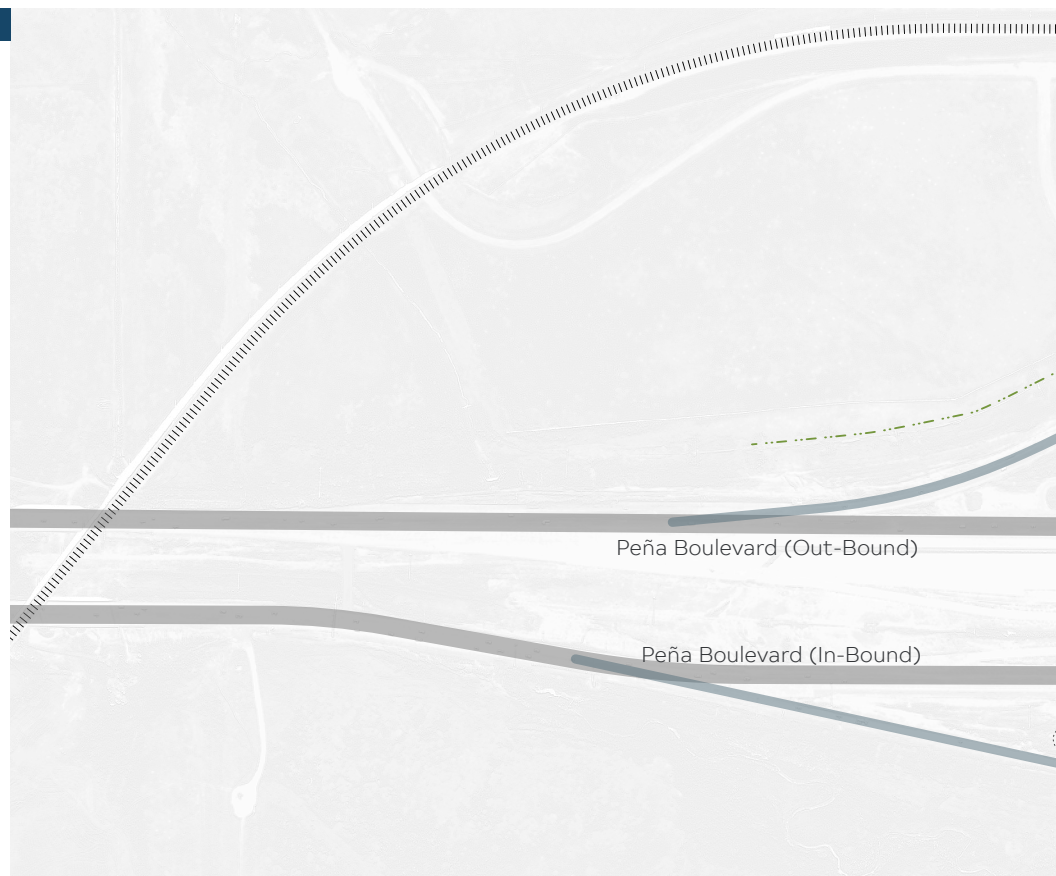
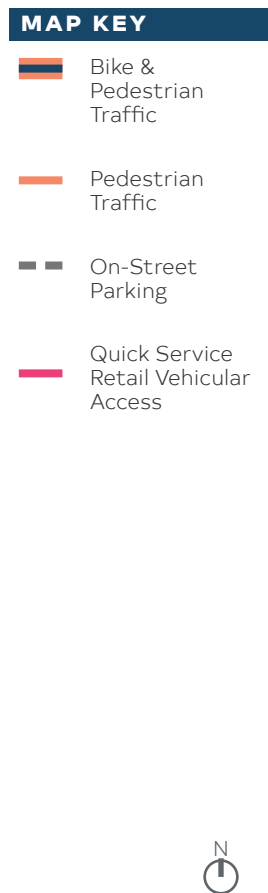
- » A maximum of 1 passenger loading zone shall be provided per block face.
- » Shuttle locations for the airport and hotels shall be provided within the District local streets connecting commercial and hotel uses to the terminal. Final locations shall be determined based on development locations.



MULTI-MODAL CONSIDERATIONS

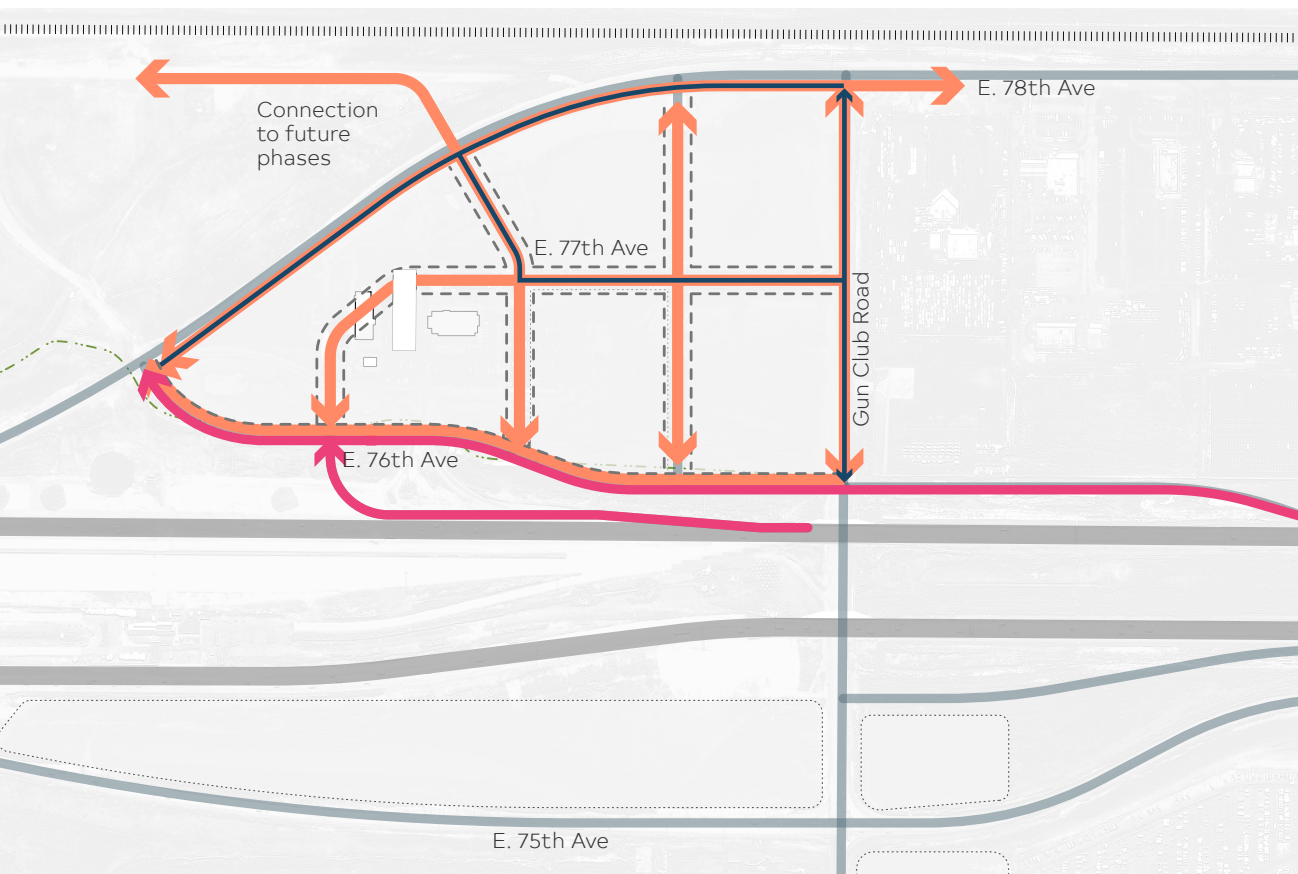
INTENT

- » Encourage bike and pedestrian mobility throughout the District and to the future development area west of 78th Avenue.
- » Accommodate bike traffic within the District per the diagram. See Chapter 04 Streetscape and Public Realm for more detail on bike facility type.
- » Encourage pedestrian traffic throughout the District with a focus on the core part of the District with the greatest amount of activity.



STANDARDS

- » Provide on-street parking within the District per the diagram.
- » Connect to future regional bike facilities.
- » Provide safe and protected bike route along 78th Avenue.










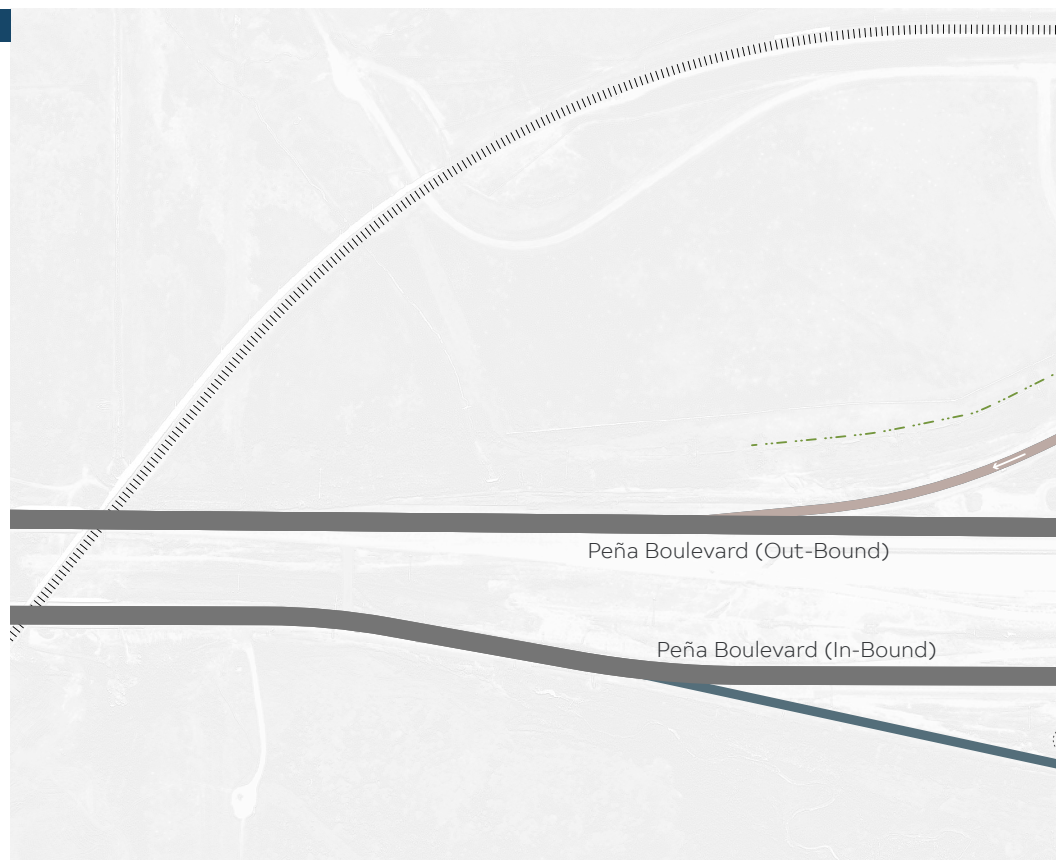
INTERSECTION STANDARDS

COLLECTOR: 78TH AVENUE, GUN CLUB ROAD

- » Curb cuts and driveways shall be positioned perpendicular to the street they serve.
- » Crosswalks and ADA ramps shall be located to connect sidewalks across intersections at 90 degrees to the direction of vehicular traffic.
- » All full movement intersections must be signalized along 78th Avenue.
- » No more than one limited or restricted movement intersection is allowed between signalized intersections.
- » No more than three intersections are allowed on E 78th Avenue west of Gun Club Road.
- » One full movement intersection shall be located to connect across E 78th Avenue to a future Phase 2 development.
- » A raised median or other DEN approved control device is required at limited or restricted movement intersections.

MAP KEY

-  Full Movement. Any turn can be made onto any road at this intersection
-  Limited Movement. One or more turning movements are restricted
-  Restricted Movement. All traffic merges into a single direction.
-  Existing Traffic Signal
-  Potential Location for Future Traffic Signal
-  Two-way traffic
-  One-way traffic



PARTIAL COLLECTOR: 76TH AVENUE

- » Crosswalks and ADA ramps shall be located to connect sidewalks across intersections at 90 degrees to the direction of vehicular traffic.
- » Stop control at intersections shall prioritize efficiency along 76th Avenue for users accessing commercial serving uses.

INTERIOR STREETS

- » Curb cuts and driveways shall be positioned perpendicular to the street they serve.
- » Crosswalks and ADA ramps shall be located to connect sidewalks across intersections at 90 degrees to the direction of vehicular traffic.
- » Vehicle access points shall provide clear views of pedestrians on sidewalks, bicyclists, and oncoming traffic.
- » Where needed, pedestrian drop-off areas shall be sited to minimize conflicts with pedestrian routes and provide direct access to primary building entrances.

GENERAL

- » Appropriate sight distance triangles shall be incorporated at all intersections.
- » Exact intersection locations, turning movements and distances between intersections shall be determined through a traffic analysis study.



BUILDING PLACEMENT & ORIENTATION

INTENT

- » Reinforce urban character and promote human scaled architecture throughout the District.
- » Enhance a sense of arrival by accentuating important district entry points and street corners.
- » Enhance the pedestrian realm by locating buildings to reinforce the street network and to frame streetscape and pedestrian zones with a consistent building edge.
- » Locate and orient buildings to articulate a pedestrian friendly streetscape, frame desired views and block visual clutter such as service and parking areas.



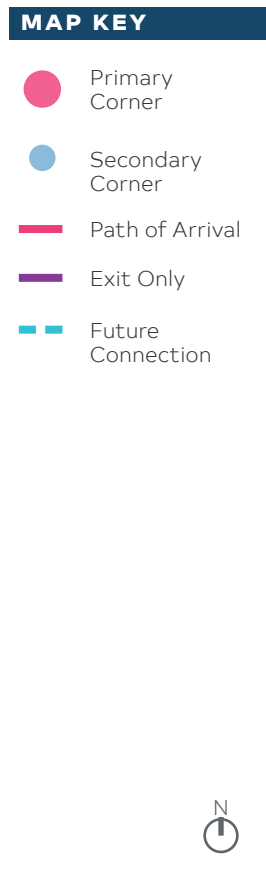
- » Encourage street facing active uses on the ground floor of buildings.
- » Maximize transparency, entrance features and architectural articulation in pedestrian facing facades.
- » Encourage the detailing of building corners, distinguish primary entries and activate building facades.
- » Reduce visibility of parking areas and minimize adjacency of parking and service areas to pedestrian areas and shared common district areas.



PRIMARY & SECONDARY CORNERS

INTENT

- » Celebrate and enhance as primary corners key locations that are:
 - Full movement intersections into the District.
 - Designated connection point to future phase 2 of the District.
 - Intersection of central organizing spines and pedestrian routes.
- » Celebrate and enhance corners located at the intersection of central organizing spines and pedestrian routes as primary corners.
- » Use architectural design and building placement to create gateways and a sense of arrival at primary corners.
- » Define and reinforce as secondary corners key locations that are:
 - Limited movement intersections into the District.
 - Key to navigation.
- » Locate buildings to frame the District and enhance navigation at secondary corners.

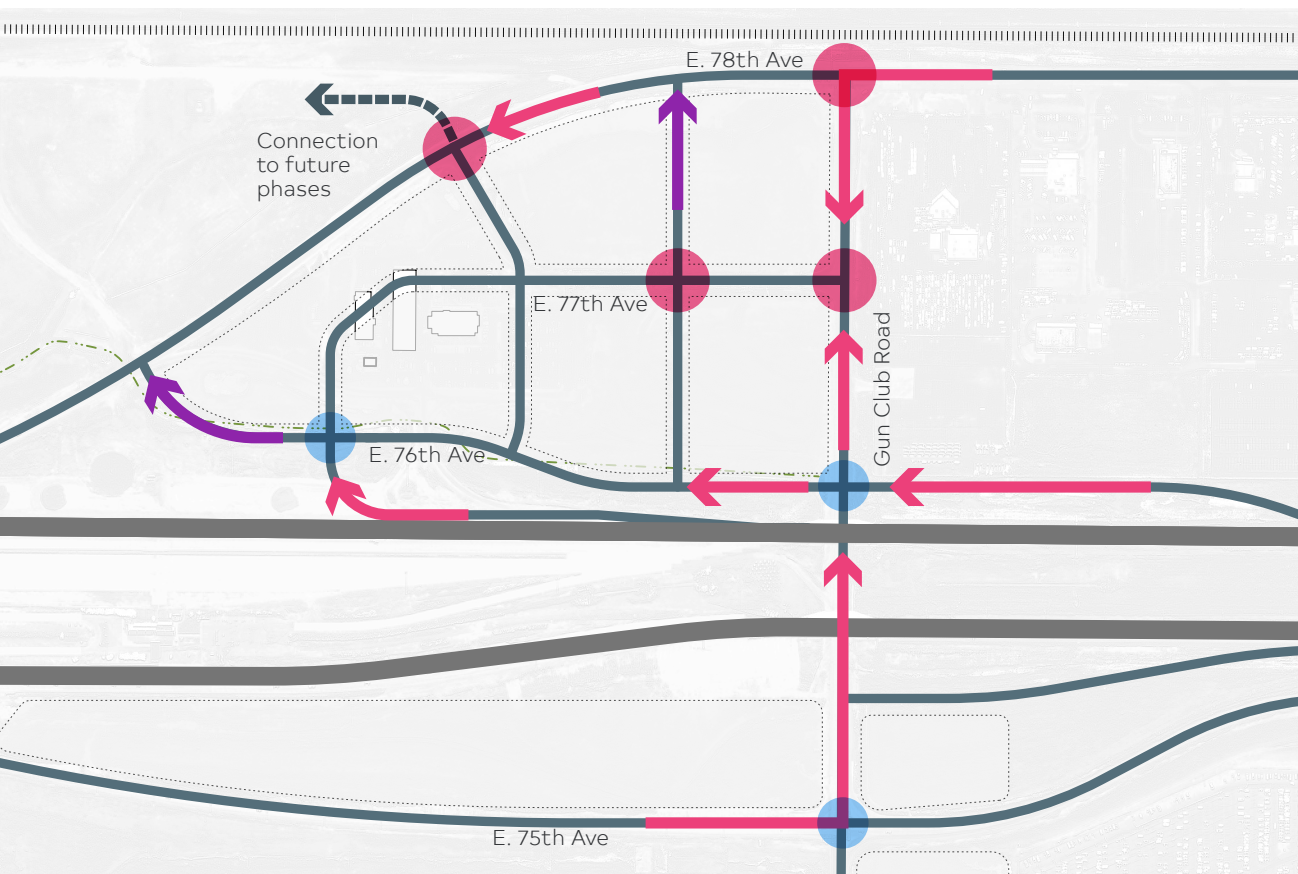


Primary Corner Standards

- » Buildings shall be located at primary corners and frontage shall wrap the corner.
- » No parking nor service areas shall be located between the building and street frontage at primary corners.
- » Back of house activities shall not be adjacent to primary corners.
- » Active uses shall be located on the ground floor at primary corners wherever possible.
- » Architectural detailing and building transparency shall be enhanced at primary corners.
- » Monumental architecture shall be encouraged at primary corners.

Secondary Corner Standards

- » Buildings shall be aligned parallel to roadways at secondary corners.
- » Parking between a building and a secondary corner shall be minimized.
- » Service areas between a building and a secondary corner shall be avoided and architecturally screened (i.e. with walls rather than landscape) where unavoidable.



BUILDING SETBACKS

INTENT

- » Clearly define the street edge.
- » Allow for variation of setbacks for streets that are more internal to the District and those that are more vehicular oriented, such as 78th Avenue and Gun Club Road.
- » Activate streets in the core of the District and encourage internal pedestrian traffic.
- » Allow separation from high traffic, airport priority streets and building edge.
- » Incorporate utility corridors in setbacks.

STANDARDS

- » Since DEN has no ROW, setbacks are measured from the back of sidewalk and amenity zone as identified in Chapter 04 Streetscapes and Public Realm.

COLLECTOR: 78TH AVENUE

- » Setbacks shall be a minimum of fifteen feet (15').
- » Setback shall include landscape elements such as groundcovers, shrubs, trees, landscape walls, hardscape, or combinations thereof.
- » Utility corridor shall be allowed within the setback.

COLLECTOR: GUN CLUB ROAD

- » No setbacks required.

PARTIAL COLLECTOR: 76TH AVENUE

- » Setbacks shall be a minimum of five feet (5').
- » Setback shall include landscape elements such as groundcovers, shrubs, trees, landscape walls, hardscape, or combinations thereof.

INTERIOR STREETS:

- » No setbacks required.



Example of landscape setback

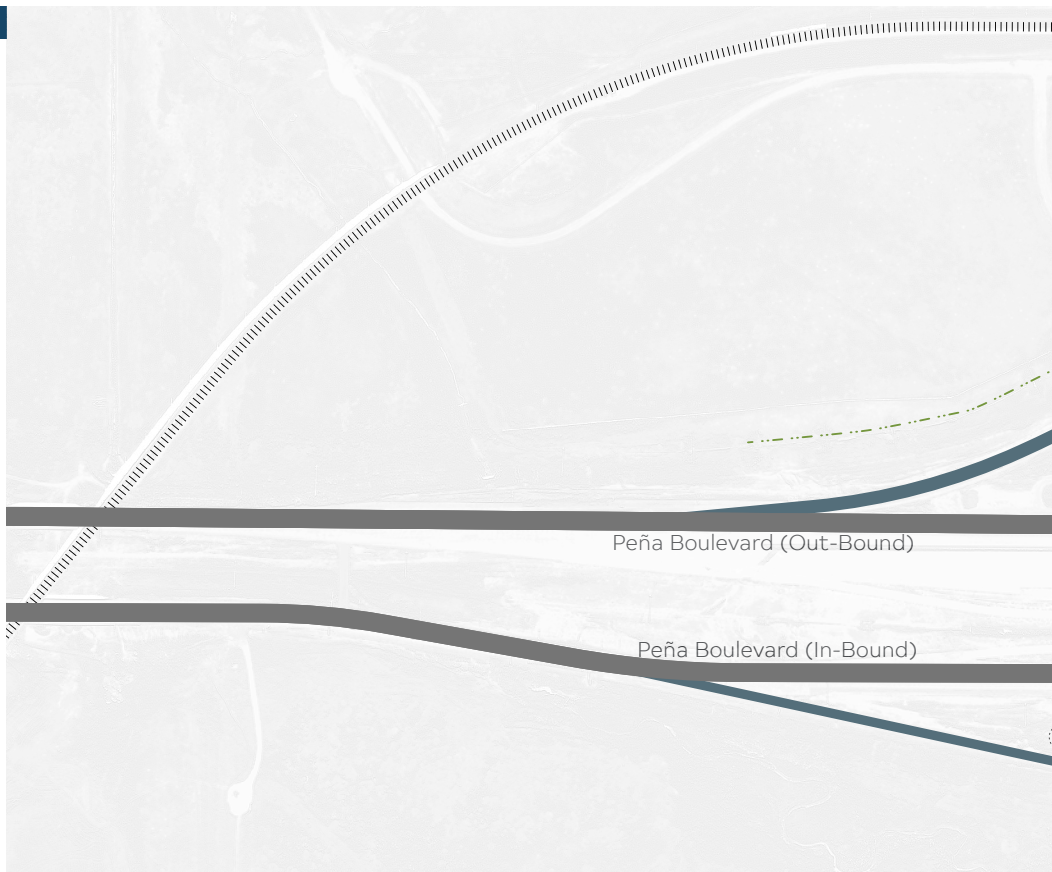
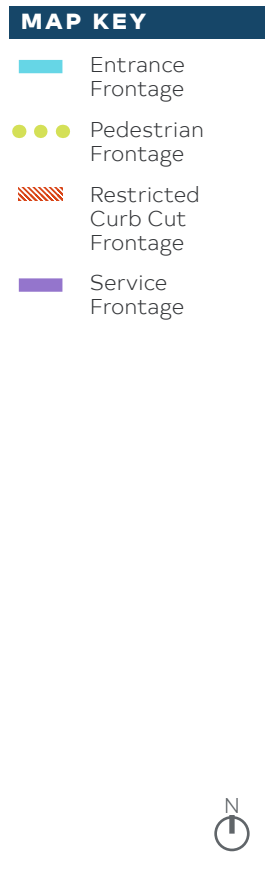


Example of no setback

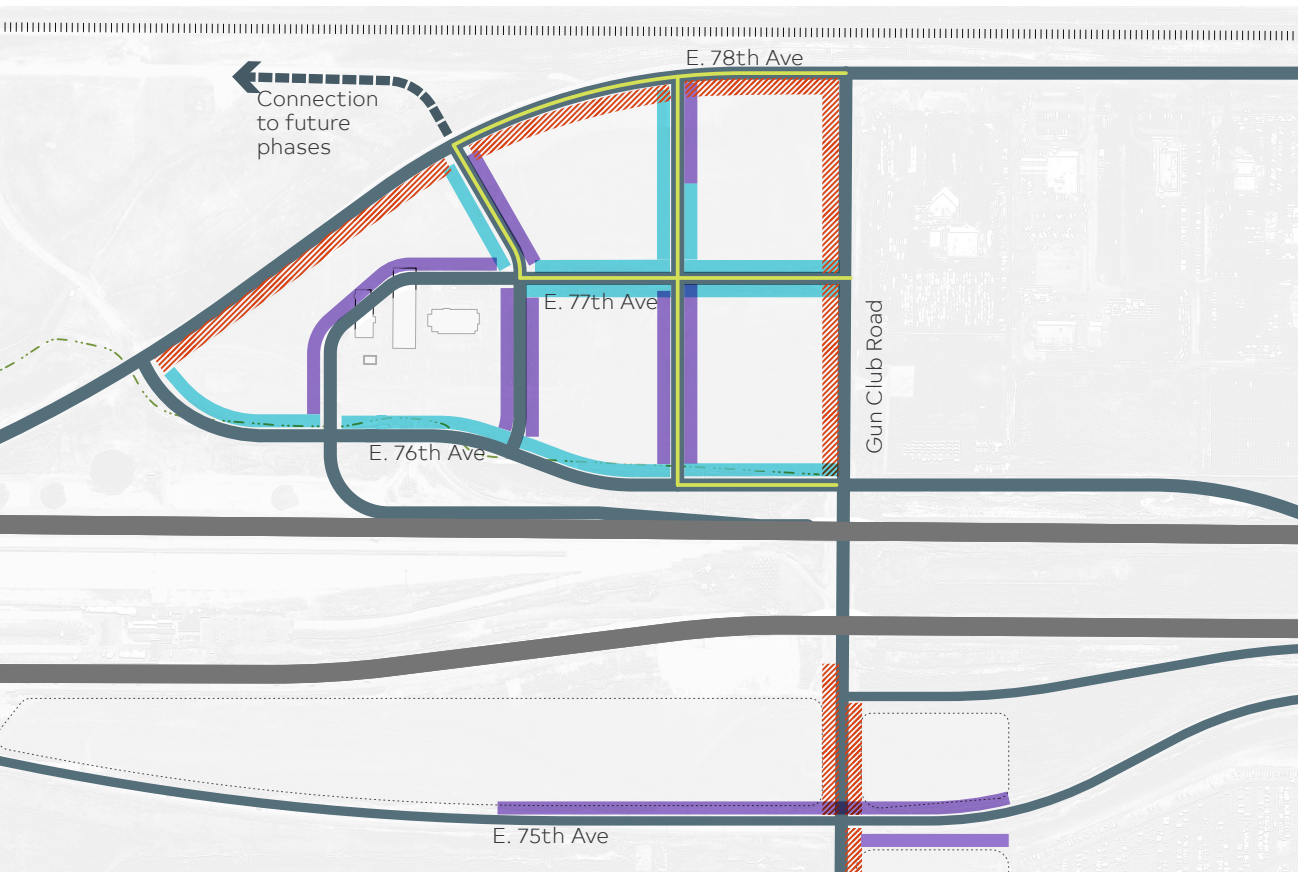
STREET FRONTAGE

INTENT

- » Designate a consistent, pedestrian scaled and welcoming street frontage that extends from the primary district entrance through a central spine connecting to future phases of the District as Entrance Frontage.
- » Designate a pedestrian scaled and welcoming street frontage that supports a community gathering area as Entrance Frontage.
- » Identify primary and continuous pedestrian routes that provide access to businesses and community gathering areas throughout the District as Pedestrian Frontage.



- » Protect the function and safe operation of airport roadways and reduce traffic disruptions on key thoroughfares through the designation of Restricted Curb Cut Frontage.
- » Provide adequate service access to businesses and buildings where it will conflict least with pedestrian and entrance activities at designated Service Frontage.
- » Provide Entrance Frontage adjacent to Central Open Space.



ENTRANCE FRONTAGE STANDARDS

- » Building entrances shall be orientated toward Entrance Frontage.
- » Buildings shall be oriented parallel to Entrance Frontage and located to maximize build-to at Entrance Frontage.
- » A pedestrian route shall clearly lead to building entrance from Entrance Frontage and be protected from vehicular traffic.
- » Service access and curb cuts shall be avoided to maximum extent possible at Entrance Frontage. Any curb cuts on Entrance Frontage shall clearly lead to building entrance and be ADA compliant.
- » Parking at Entrance Frontage shall be minimized and architecturally screened from frontage (i.e. wall rather than vegetation screen). Buildings shall be located to screen parking from frontage to maximum extent possible.
- » Retail uses within buildings shall be visible and provide clear entrance from Entrance Frontage.

PEDESTRIAN FRONTAGE STANDARDS

- » Buildings shall be located to maximize build-to at pedestrian frontage.
- » Curb cuts and service access shall be minimized at pedestrian frontage and be ADA compliant.
- » Parking shall be minimized at pedestrian frontage and screened from frontage.

RESTRICTED CURB CUT FRONTAGE STANDARDS

- » Vehicular access and curb cuts shall be avoided on E 78th Avenue and Gun Club Road.
- » Any pedestrian access from E 78th Avenue and Gun Club Road shall be secondary to the main entrance.

SERVICE FRONTAGE STANDARDS

- » Curb cuts shall be shared between uses where possible to reduce the number of required curb cuts.
- » Service areas shall be screened from street frontage.
- » ADA compliant paths shall be provided across all curb cuts.
- » Curb cut design shall conform to Denver's Department of Transportation and Infrastructure standards for commercial access.

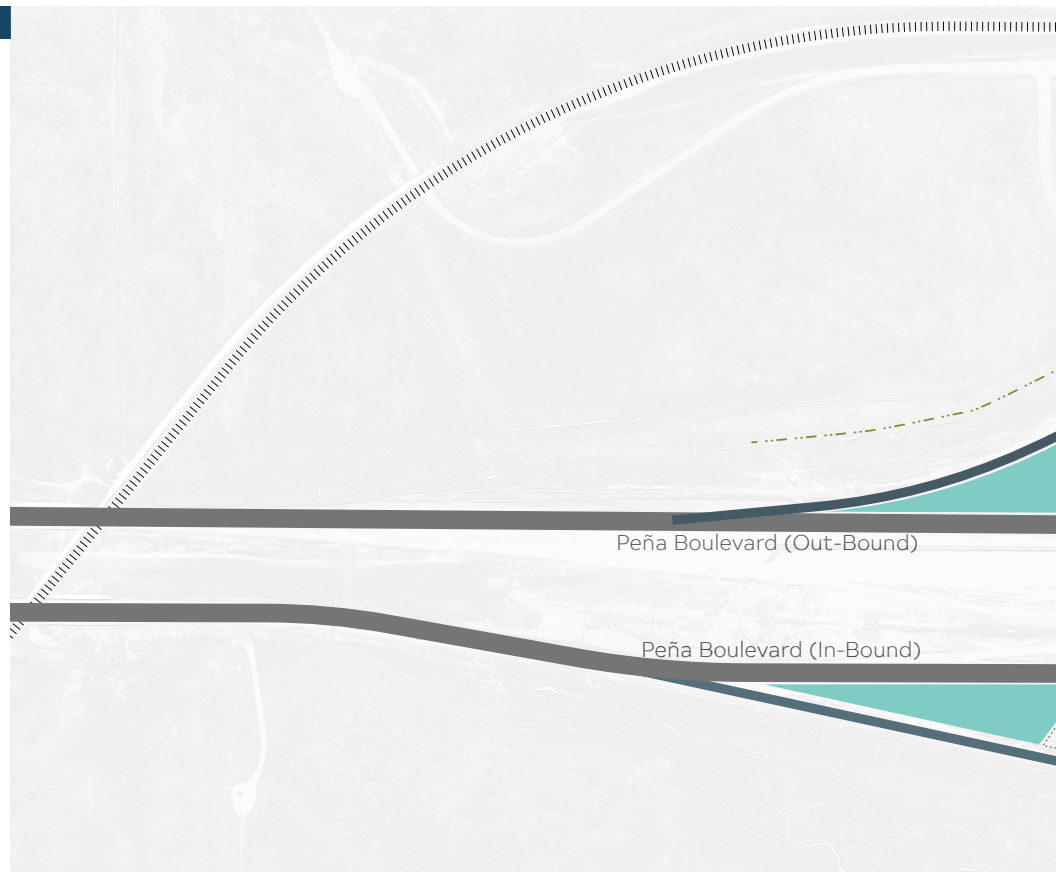
OPEN SPACE FRAMEWORK

INTENT

- » Promote the character of the prairie ecosystem in which the airport is situated.
- » Position open space as an organizing element to enhance district character.
- » Maintain accessibility of open space to all district visitors.
- » Transition from edge condition to district, either by buffering the District from negative influences or highlighting the District for outside visibility.
- » Connect open space to airport mobility networks and to natural resources such as riparian corridors.

MAP KEY

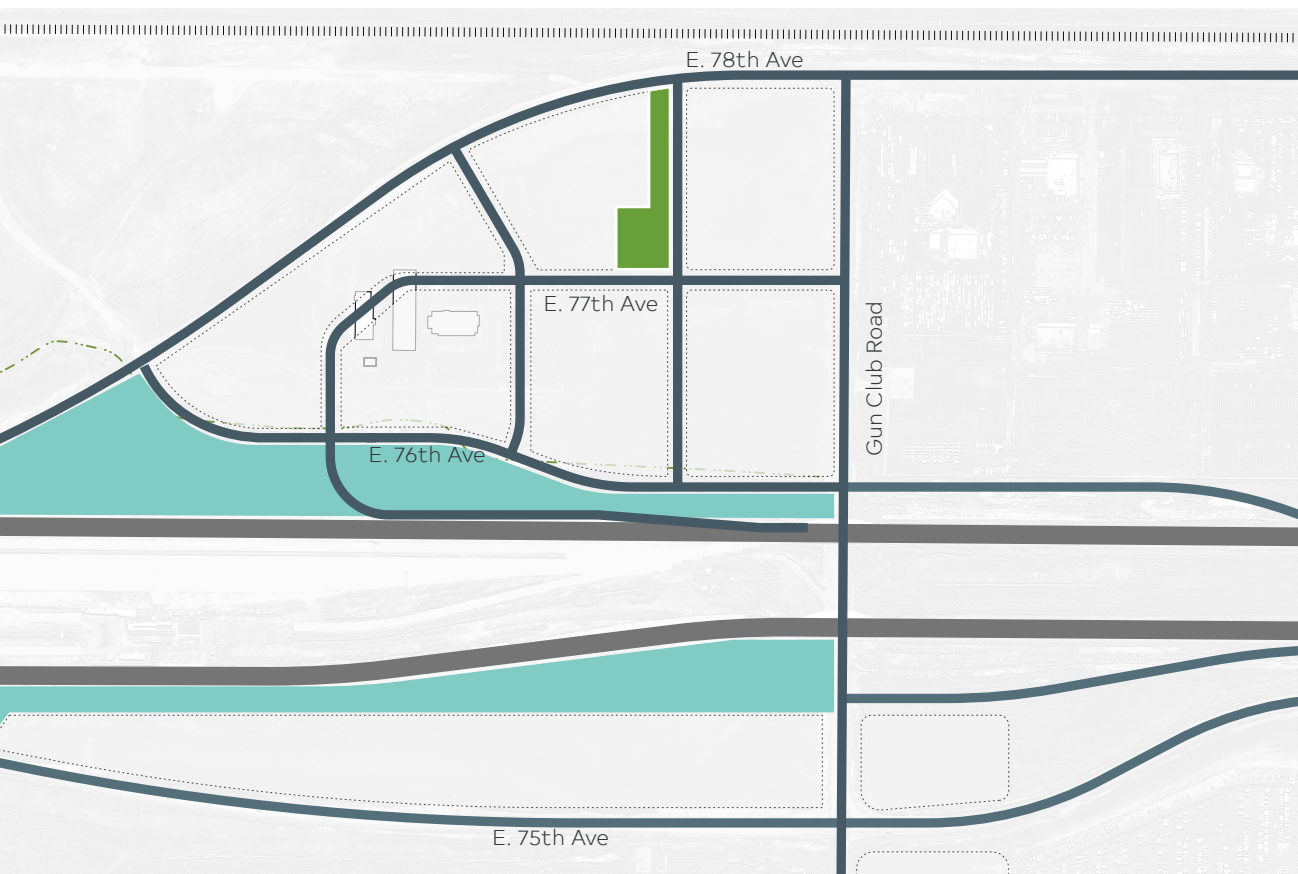
- Peña Buffers
- Central Open Space



Note: Exact location of central open space to be determined as development occurs.

STANDARDS

- » Each parcel development shall contribute to the District central open space.
- » West Approach is within the 10,000' critical area for Wildlife-Attractant' Separation Area for the final build-out of future DEN Runways, as defined by the Federal Aviation Administration (FAA). Planting shall comply with DEN's Wildlife Hazard Management Plan. The density and location of trees and landscape must be reviewed and approved by USDA.
- » Plant species selection shall conform with the approved plant list in DEN's Wildlife Hazard Management Plan. Any plant species not on the list must be submitted to USDA for review and approval.



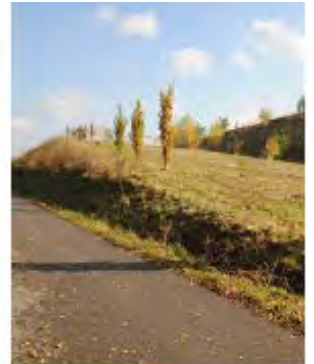
PEÑA BUFFERS

INTENT

- » Provide attractive and inviting foreground to district from Peña Boulevard.
- » Mitigate traffic noise and highway visibility from district development.
- » Provide visual interest through color and texture of plants and variation through landforms.
- » Frame desirable views into the District.
- » Screen undesirable views into the District.

STANDARDS

- » Plant species selection shall be coordinated and approved by DEN and the USDA.
- » If desired, space for pedestrians shall be provided above critical stormwater conveyance area.
- » Any alteration to the existing stormwater channel shall be coordinated with DEN's Division of Airport Infrastructure Management.
- » Berming shall be considered as a buffering solution in lieu of tree or shrub planting.



CENTRAL OPEN SPACE

INTENT

- » Provide centrally located open space within the District for gathering and outdoor enjoyment.
- » Connect district core to natural resources and pedestrian circulation system extending across the airport property.
- » Highlight and promote views to airfield and airport operations.
- » Create a strong indoor-outdoor relationship between the adjacent buildings and central shared space.

STANDARDS

- » Central open space shall be located north of 77th Avenue and south of 78th Avenue buffer and to the west of the north-south road running through the District.
- » Central open space shall be a contiguous space and a minimum of 35,000 SF.
- » Solar exposure shall be maximized.
- » Active uses shall be located on the ground floor adjacent to the central shared space.
- » Building transparency shall be maximized to the greatest extent possible for adjacent uses.
- » A variety of seating options shall be provided accommodating both large and small groups.
- » Shade and visual interest shall be provided through trees, plantings and other architectural features.
- » Pedestrian connections shall be provided to the multi-use path along 78th Avenue.

SUSTAINABLE DISTRICT SYSTEMS

INTENT

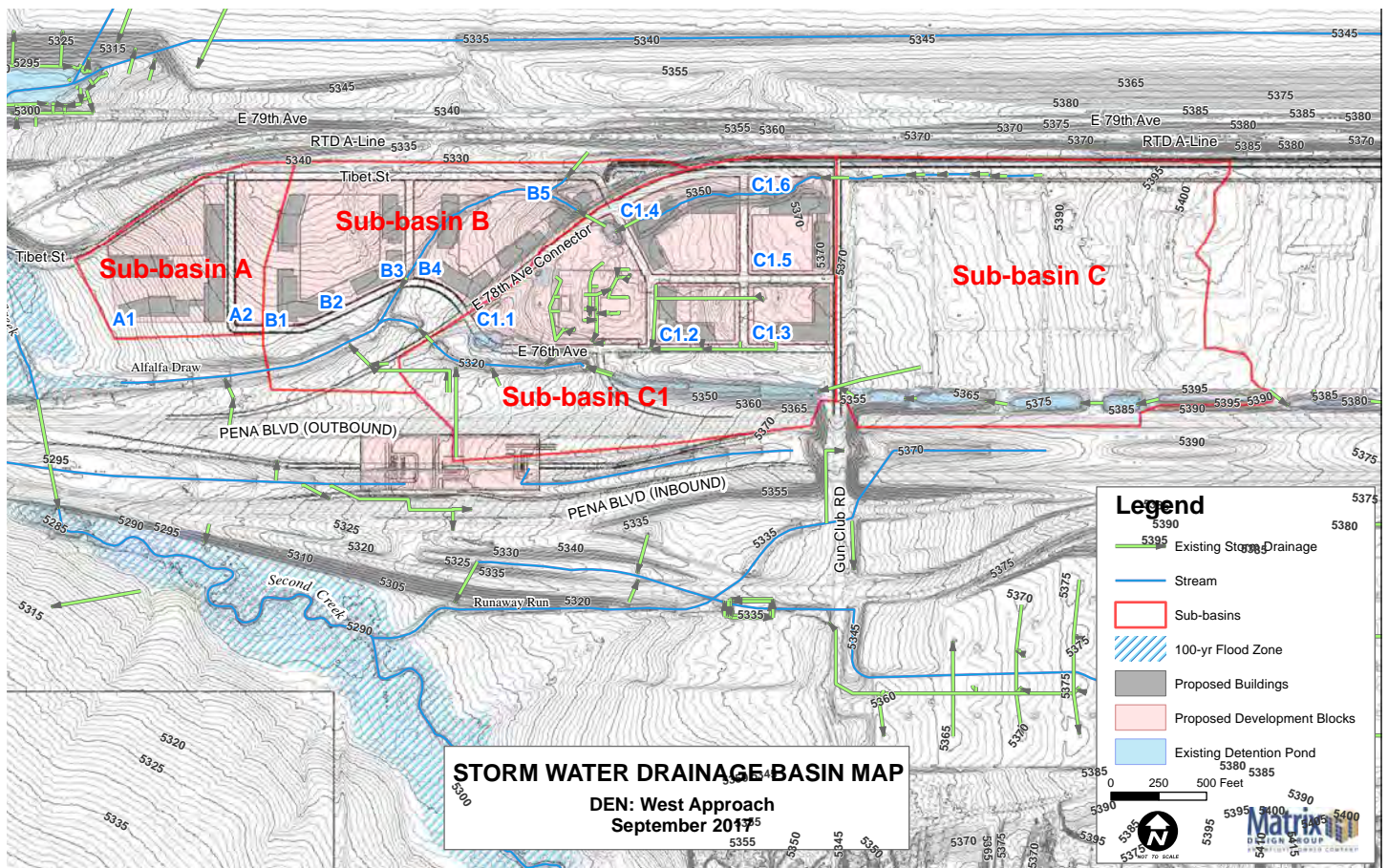
- » Incorporate best practices into sustainable district systems that leverage the impact of environmental stewardship.
- » Preserve the ability to take the most sustainable action as infrastructure and technology innovations evolve.
- » Advance DEN's goal to be the most sustainable airport in the world and align with the City's greenhouse gas reduction goals.
- » Improve building performance and reduce operations and maintenance costs.
- » Reduce carbon emissions, energy usage and environmental impact during construction and long-term operation.
- » Reduce water usage in both interior and exterior applications.
- » Provide a flexible framework that allows each project within the District to employ the best practices and most up-to-date technology available at the time of its construction.

STANDARDS

- » Water quality and detention shall be handled by DEN's regional detention system for all sub-basins except for that which is located in the westernmost portion of phase 2, marked "Sub-basin A" on figure 5.1. All infrastructure to convey storm water to the regional system shall conform with DEN's Design Standards Manual, Drainage Master Plan and Water Quality Master Plan.
- » Water quality and detention for the limited run-off not handled by DEN's regional detention system shall be handled with bio-swales, street side stormwater planers, bio-retention tree trenches, permeable pavers in alleyways or other best practices in coordination with DEN's division of Airport Infrastructure Management.
- » Utilities corridors shall be designated for running new utilities with appropriate clearances and connecting new utilities to existing collectors and mains.
- » Curb inlets shall be used on interior streets and tie-into existing system or replace existing regional inlets where appropriate.
- » Granular bedding shall not be used for any culverts or pipes. Flow fill bedding shall be used.
- » Storm drainage shall utilize reinforced concrete culvert pipe in a minimum 24" dimension.

- » Areas for snow storage shall be indicated on all site plans.
- » All projects shall adhere to the Denver Green Code.
- » All buildings shall meet LEED Gold.
- » All buildings shall comply with Denver Green Roof Ordinance.
- » All projects will be 100% electrified; gas infrastructure will not be provided or permitted.

Figure 5.1 Proposed Storm Drainage



BUILDING & PARCEL DESIGN

BUILDING & PARCEL DESIGN

48 BUILDING CHARACTER

50 MASSING & SCALE

51 ROOF FORMS & PARAPETS

52 GLAZING & WINDOWS

53 EXTERIOR MATERIALS

54 VEHICULAR ACCESS AND PARKING

56 SITE LANDSCAPE

58 SERVICE, DELIVERY, STORAGE & LOADING

Architecture should accurately express use. The ground floor of these architectural types must be activated, primarily transparent and pedestrian scaled to enhance the experience at West Approach. Building form and relation to landscape should enhance views to the airfield and Front Range and from Peña Boulevard. Building orientation should address the street, take advantage of prevailing summer winds, and buffer against winter wind conditions.



BUILDING CHARACTER

INTENT

- » Reflect DEN Design Principles in [Architectural Design Standards Manual](#) (DSM).
- » Align with DEN's Sustainability Design Standards Manual and standards in Chapter 02 District Layout Sustainable District Systems.
- » Express sustainability through building character, building performance and honest expression of the design.
- » Design buildings to be aligned with DEN's architectural character but not replicating character of terminal and other iconic DEN buildings.
- » Promote a clear hierarchy of pedestrian entries.



- » Encourage architecture that is distinctive in character, is harmonious, and clearly expressive of its purpose.
- » Encourage thoughtfully detailed buildings of a variety of styles and materials, for which all visible sides and roofs are given equal design consideration.
- » Prioritize the ground story of buildings, and activate them with elements (e.g. windows, doors, awnings, patios, colonnades) that enhance the pedestrian experience.
- » Enhance important or desirable building features and conceal less important or less desirable building features.
- » Identify primary entries with facade articulation, plazas, overhead cover, and landscaping.
- » Design buildings in response to site conditions in order to contribute to an integrated streetscape/landscape.



MASSING & SCALE

INTENT

- » Visually break down massing by promoting scale and height changes in the overall building massing and between adjacent buildings.
- » Promote variation, human scale and interest.
- » Provide solar penetration to ground level for all buildings within the District.
- » Articulate building massing and roof lines to avoid expansive straight lines.

STANDARDS

- » Long facades (i.e. over 150') shall be articulated by utilizing material changes, shallow projections and recesses, and fenestration details. Provide shifts in the building footprint additively and subtractively. Uninterrupted wall surface at a single height or in a single plane exceeding 50' are not permitted.
- » Wall surface planes larger than 15,000 SF shall be provided with facets, recesses or projections that break the flat facade into visually separate parts and shall be of sufficient dimension to create depth and variation of light and shadow.
- » The ground level building facades shall have human-scaled details that are integrated into the architecture of the building to enhance pedestrian comfort and provide visual interest.
- » Provide a taller floor-to-ceiling height at the ground floor than floors above.
- » Building facades at primary corners shall be enhanced by distinctive material and color, increased percentage of glazing, and/or significant architectural features.
- » Articulate buildings both vertically and horizontally with scaling elements such as material, color, and/or fenestration changes, variations in parapet walls, terraces, and/or employing upper story step backs.



ROOF FORMS & PARAPETS

INTENT

- » Include substantial soffits and fascia that add architectural character with shadows and massing.
- » Design roof forms and parapets that enhance architectural interest and articulate scale and massing of the architecture.

STANDARDS

- » Roof lines shall reflect the variations in building massing and respond to the overall scale of building facades.
- » The eave line shall have variation in height, profile and scale.
- » Rooftop mechanical equipment shall be screened from street view and integrated into the overall building design.
- » Consider designing roofs for accommodating solar in the future, if it is not initially implemented.
- » Roof line shall be true to the architectural style.
- » Clay tile and asphalt shingle roofs shall not be permitted.

GLAZING & WINDOWS

INTENT

- » Prioritize the ground story of buildings, and activate them with elements (e.g. windows, doors, awnings, patios, colonnades) to create a pedestrian oriented environment.
- » Maximize transparency of windows to activate the street and create visual and architectural interest.
- » Comply with FAA safety requirements regarding glare to aircraft.
- » Consider passive solar design through architectural glazing.

STANDARDS

- » All windows shall be trimmed with architecturally appropriate details in keeping with the overall style of the building's architecture.
- » Transparency shall be, at a minimum, 40% on building facade that features the primary entrance, and 25% at side and rear facades.
- » Ground floor retail shall target 75% transparency.
- » Windows shall be recessed from the building facade at 3" minimum to provide a shadow line. Flush windows are not allowed with the exception of storefront or curtain wall systems.
- » The use of tinted glass will be reviewed with the submission of a glass sample.



EXTERIOR MATERIALS

INTENT

- » Utilize materials and colors that complement DEN's high plains environment.
- » Build with durable, low-maintenance materials that withstand the effects of time and contribute to the long term viability of the District.
- » Include variations in materials that provide human interest, scale and variety.
- » Combine The American New West idea with a blend of contemporary architecture.

STANDARDS

- » Materials shall be located on the facade such that they enhance the architectural style, massing and detail.
- » Combinations of different materials and/or textures shall be used on each building facade to create character and interest through the interaction of texture and color.
- » Buildings shall be constructed with durable, low-maintenance materials that withstand the effects of time.
- » Material transitions shall occur on inside corners or at architecturally significant locations.
- » Glass block and EIFS shall not be used.
- » Minimize the use of stucco. When used, incorporate reveals in its detailing.
- » Synthetic materials shall be used in ways that reflect their intrinsic character and will be reviewed with the submission of a physical sample.
- » Consideration shall be given to the design of ledges, canopies, and other areas such that bird nesting and perching areas are not created.



VEHICULAR ACCESS AND PARKING

INTENT

- » Promote a walkable, pedestrian friendly experience while providing adequate private and commercial vehicle access.
- » Minimize conflicts between vehicles, bicycles, and pedestrians and enhance pedestrian and bicycle safety.
- » Minimize the visual impact of parking and vehicular activity and reduce trespass of headlights, lighting and vehicle noise onto adjacent properties and streetscape.
- » Encourage multi-modal mobility options and environmental mitigation strategies.

STANDARDS

- » Parking and access areas shall meet ADA requirements.
- » Refer to Chapter 02 District Layout for block configuration, intersection and street standards.
- » Curb cuts and drives and ramps shall cross the streetscape at right angles and be perpendicular to public streets.
- » Vehicular and service access shall be limited to a maximum of 2 points for each parcel along the long side of the parcel face and 1 point of access along the short side of the building parcel and shall be coordinated with adjacent and opposing development. Shared access points between individual parcels, if applicable, is encouraged.
- » Parking shall be minimized at the Entrance Frontage and screened using architectural features.
- » Side streets and alleys shall be used for parking access to the greatest extent feasible.
- » Surface parking shall be screened from the street and adjacent uses by landscaping, screen walls or fences along entire length. Screening shall be sufficient height and density to block headlights.
- » Large surface parking areas (greater than 75 spaces) shall be broken up into smaller increments or "pods" with interior landscaping (e.g., landscaped islands or medians) and provide clearly marked pedestrian connections.
- » Surface parking shall be paved with concrete, asphalt, or approved permeable pavement.

- » Parking and drives associated with a porte-cochere or drop-off area shall be demarcated with enhanced paving such as colored concrete, stamped concrete and/or pavers.
- » The number of parking spaces reserved for electric vehicles shall be determined by requirements within Denver Building Code.
- » Shared parking is encouraged for complimentary programmatic uses to reduce parking demand in the District.
- » Secure on-site bicycle storage shall be provided. Exterior bike racks shall be located in a location visible from building entrances or windows.
- » Covered parking shall leverage opportunities to capture solar energy.



SITE LANDSCAPE

INTENT

- » Minimize habitat opportunity, food sources and other attractants of wildlife that pose a risk to aircraft operations, with special attention to species of concern identified by the USDA in the Wildlife Hazard Management Plan.
- » Coordinate with other DEN natural resources and landscape strategy guiding documents in the future.
- » Create a consistent character appropriate for West Approach site and high plains environment.
- » Soften the massing, scale and visual impact of buildings.
- » Locate and design landscaped areas to enhance important views from West Approach to the Front Range and airfield and from Peña Boulevard into West Approach.

STANDARDS

- » Locate enhanced landscape treatments at identified gateways or key intersections to highlight district entry points and reinforce district identity.
- » Landscaping shall be utilized to accentuate and define building entrances and visually frame buildings.
- » Locate and select plant material to soften enclosure walls and buffer parking, service, delivery, storage, and loading areas.
- » Planting and irrigation design shall account for climatic exposure and unique watering conditions. Sun and shade conditions and water use shall be considered when selecting plant material.
- » The use of plant material shall be responsive to the prevailing winds of the site. Plant materials shall be used to create windscreens, protect building entries, and outdoor areas.
- » All landscapes shall incorporate a mixture of deciduous and evergreen plants with plants that are flowering and have good fall color or winter interest to provide a four-season landscape.
- » Landscape design shall adhere to any view plane requirements.

- » West Approach is within the 10,000' critical area for Wildlife-Attractant' Separation Area for the final build-out of future DEN Runways, as defined by the Federal Aviation Administration (FAA). Planting shall comply with DEN's Wildlife Hazard Management Plan. The density and location of trees and landscape must be reviewed and approved by USDA.
- » Plant species selection shall conform with the approved plant list in DEN's Wildlife Hazard Management Plan. Any plant species not on the list must be submitted to USDA for review and approval.



SERVICE, DELIVERY, STORAGE & LOADING

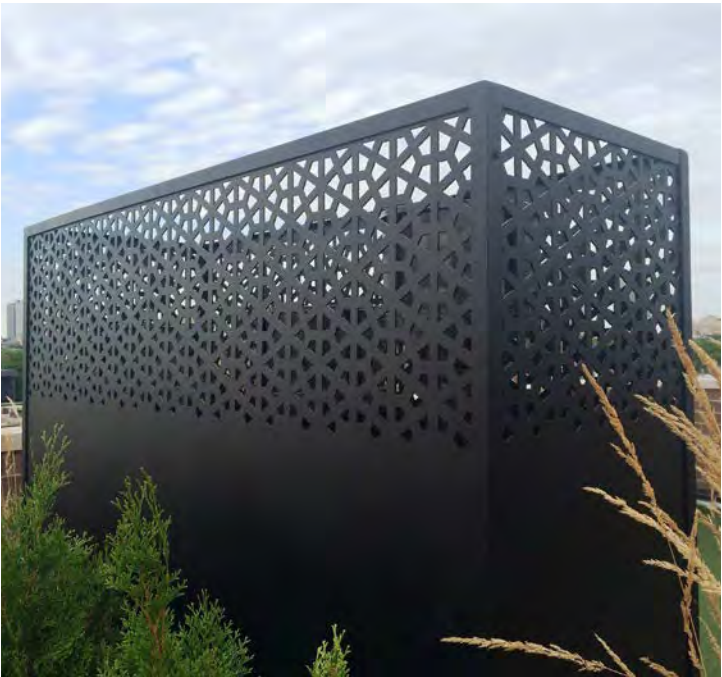
INTENT

- » Minimize the visual and auditory impact of service, delivery, storage and loading areas on pedestrian activity and the public realm.
- » Utilize architectural elements to screen undesirable utility and service use functions.
- » Separate service and utility areas from main entry points into buildings.

STANDARDS

- » Service, delivery, storage, and loading areas shall not be located on the same side of a building as the primary pedestrian entrance.
- » Vehicular access to service areas shall not be located on the same facade as the primary pedestrian entrance.
- » Locate service areas internal to the building or use building to screen service areas when possible.
- » Walls or enclosures shall be constructed to screen exterior service areas from streets, pedestrian corridors, parks, open space, and building entries.
- » Screening walls and enclosures shall be incorporated into the building architecture or use materials, colors and styles consistent with the associated building. Plant material shall not be used as an architectural screen.
- » Walls that exceed 4' in height must be 40% open from 4' – 7' except where screening unsightly service or utility components.
- » All utilities, their connections, meters and exterior mounted mechanical equipment shall be located out of view of streets, completely screened, or located underground as permitted by the utility provider or other regulations.
- » Materials, equipment, finished/semi-finished products may only be stored outside of buildings in screened enclosures.
- » When service, delivery or loading access abuts or crosses a publicly accessible sidewalk, walkway, or pedestrian-only corridor, the pedestrian connection shall be continued and given priority over the loading access by paving materials which continue the sidewalk.

- » Service, delivery, and loading access shall not cross the central open space.
- » Where multiple buildings are proposed, service areas shall be combined to the extent possible and the number curb cuts shall be minimized to reduce the conflict between pedestrians, and cars and service vehicles.
- » Trash areas shall include area for recycling and other needs such as composting.
- » Adequate space shall be provided to allow for required loading, including maneuvering, to take place completely out of a public or private streets.



STREETSCAPES & PUBLIC REALM

STREETSCAPES & PUBLIC REALM

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80 IRRIGATION

81 GRADING & DRAINAGE

The streets at West Approach create an efficient network of circulation that promotes connectivity, walkability and direct access for all modes of transportation. For the purpose of these Standards, streets within the West Approach are designated as: 78th Avenue, Gun Club Road, 77th Avenue, 76th Avenue, and interior streets. These designations describe street functionality, general dimensions, and form. Existing streets that have a unique relationship to the West Approach site are 78th Avenue and Gun Club Road. Since they are existing streets, the level of improvement required for development within the West Approach will be determined by DEN. The following street cross sections generally illustrate the design intent and width standards of designated street typologies.

The streetscape, which is located within the larger street, encompasses the space from building face or parcel edge to the curb line of the roadway. It is the primary area of occupation by the pedestrian and, as such, has significant power to shape his or her experience by promoting walkability and a comfortable walking environment. Shade, protection from wind and seating areas in which to stop and rest or re-orient activate the streetscape by increasing its use and are as important as clear through walking zones. Individual parcel improvements that abut listed street typologies are harmonious and consistent with the streetscape improvements.



STREETSCAPE

INTENT

- » Provide clear entry points and gateways to the site from critical intersections.
- » Create a walkable, pedestrian friendly street that minimizes conflicts between bicycles, pedestrians and vehicles.
- » Provide a high-quality streetscape network and unify the streets and public realm with a unique character.
- » Encourage pedestrian activity on the sidewalk and to support adjacent activities such as shopping, dining and gathering.
- » Create a cohesive rhythm with the location of landscape plantings, furnishings, paving and lighting.

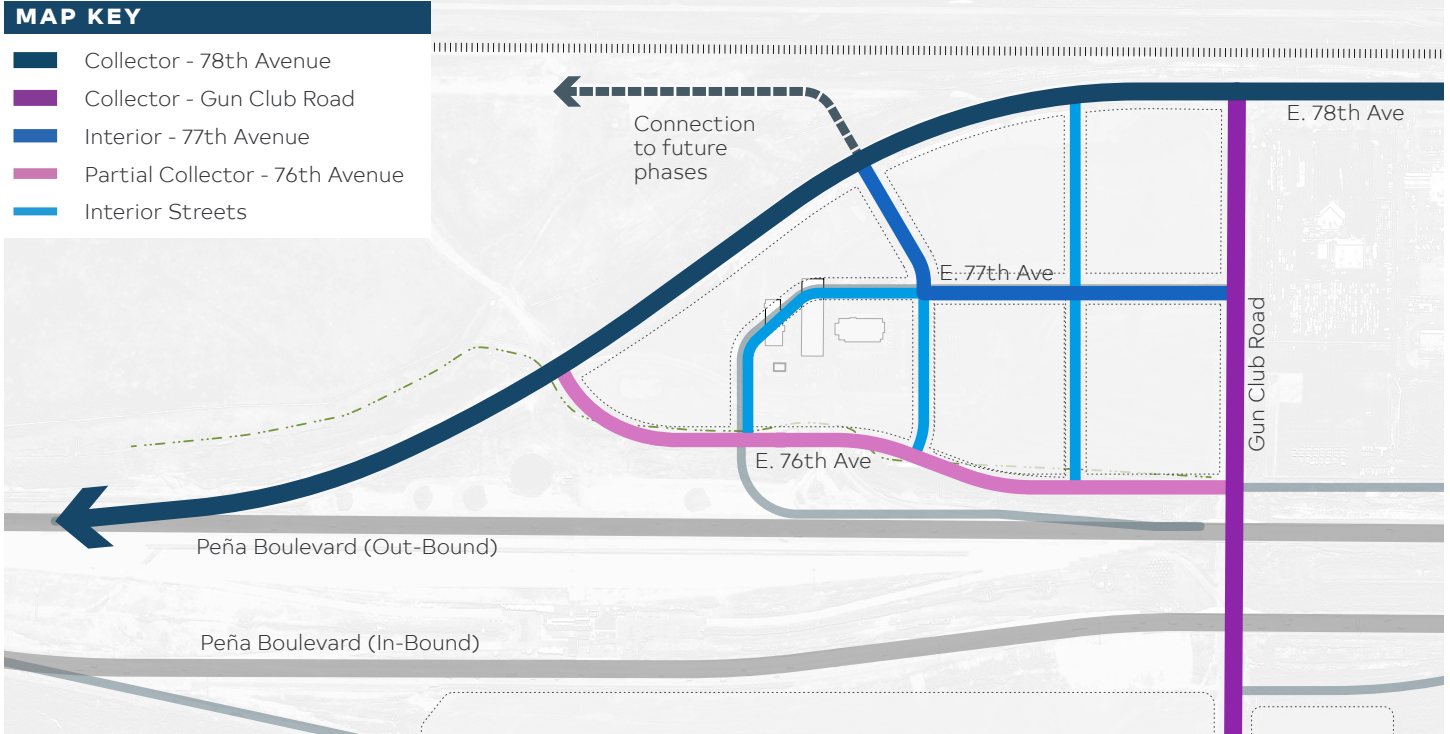
STANDARDS

- » Pedestrian walking zones and multi-use paths shall be unobstructed and clear at all times.
- » Street furnishings shall include benches, bike racks, trash and recycling receptacles.
- » No private project improvements shall be located within the streetscape except for driveway and utility access and approved site furnishings.
- » Streets shall be landscaped with plantings to create a cohesive and unified character.
- » Seating, paving patterns, signage and lighting shall be used to further accentuate the human scale of development along the streetscapes especially at building entries.
- » Street furnishings, trees and amenities shall occupy consistent, well defined amenity zones parallel to the pedestrian walking zone.
- » Roadway intersections are encouraged to extend out ("bulb" or "bump") with a specialty corner treatment to support pedestrian cross traffic.
- » Across the entire District, all internal streets shall be visually cohesive through the use of similar or complimentary streetscape elements, hardscape treatments, and plantings.
- » Sidewalks shall provide safe, comfortable and direct pedestrian connections.
- » Trees shall be located such that a continuous overlapping canopy is not created.
- » No fruit producing trees shall be utilized.

STREET TYPES

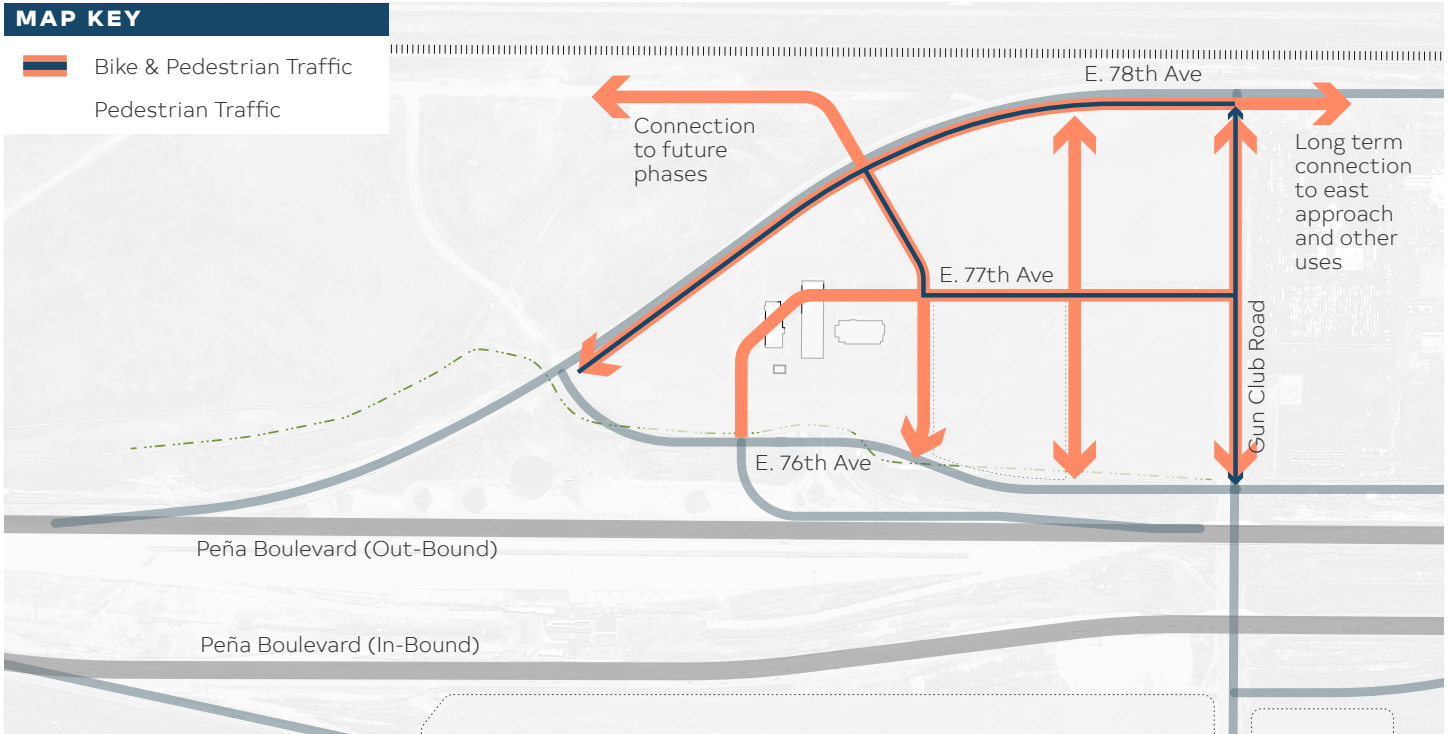
MAP KEY

- Collector - 78th Avenue
- Collector - Gun Club Road
- Interior - 77th Avenue
- Partial Collector - 76th Avenue
- Interior Streets



MAP KEY

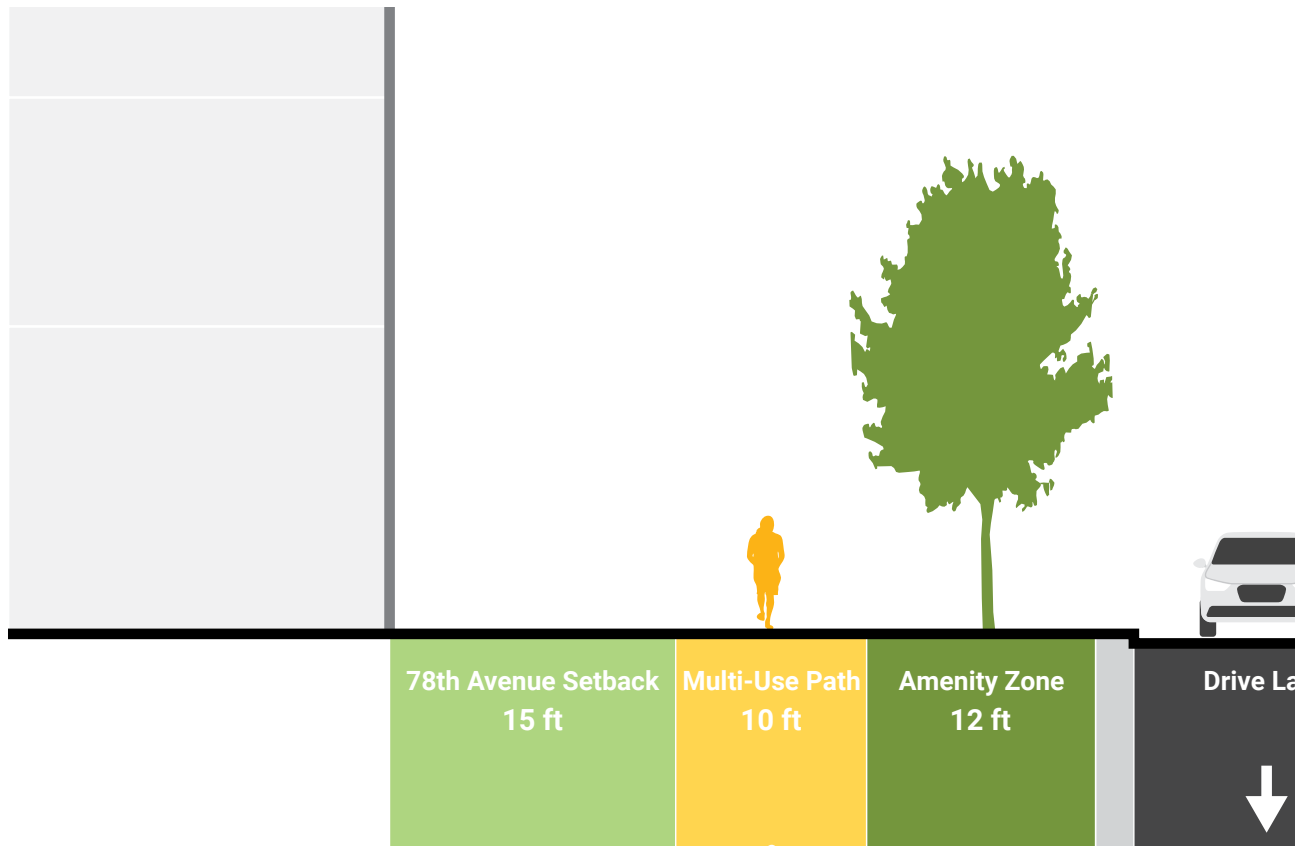
- Bike & Pedestrian Traffic
- Pedestrian Traffic



COLLECTOR: 78TH AVENUE

INTENT

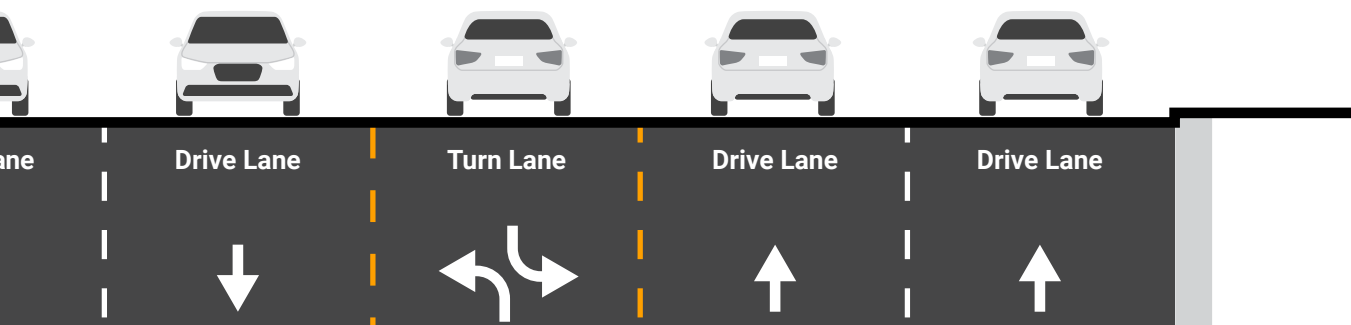
- » Reinforce 78th Avenue as a major east-west connection and maximize it's efficiency for airport operations.
- » Retain the primary means of egress from airport facilities to out-bound Peña Boulevard.
- » Prioritize safety and minimize full movement intersections as 78th Avenue approaches Peña Boulevard.
- » Plan for a full movement intersection to connect to future development west of 78th Avenue, per District Layout standards.
- » Enhance multi-modal connectivity with a two-way multi-use path.



STANDARDS

- » Illustrative section — (see section below) shall be representative of 78th Avenue between Gun Club Road and 76th Avenue.
- » Curb cuts and vehicular access to parcels shall not be permitted from 78th Avenue.
- » The streetscape shall encompass a minimum of twenty-two feet (22') including a twelve feet (12') Amenity Zone and ten feet (10') Multi-Use Path.
 - A clear Multi-Use Path shall be organized to create a continuous unobstructed bike and pedestrian zone, a minimum of ten feet (10') wide, for ease of travel and maintenance.
 - An Amenity Zone shall be organized to create a continuous area, a minimum of twelve feet (12') wide and shall contain streetscape elements such as signage, benches, trash and recycling receptacles, and bike racks.
- » 78th Avenue setback shall be provided in addition to the streetscape standards and in accordance with Chapter 02 District Layout.
- » Tree plantings shall be coordinated with utility corridor to ensure that there are no conflicts.
- » No on-street parking shall be provided.
- » Turn lanes and acceleration lanes shall be provided based on traffic needs.

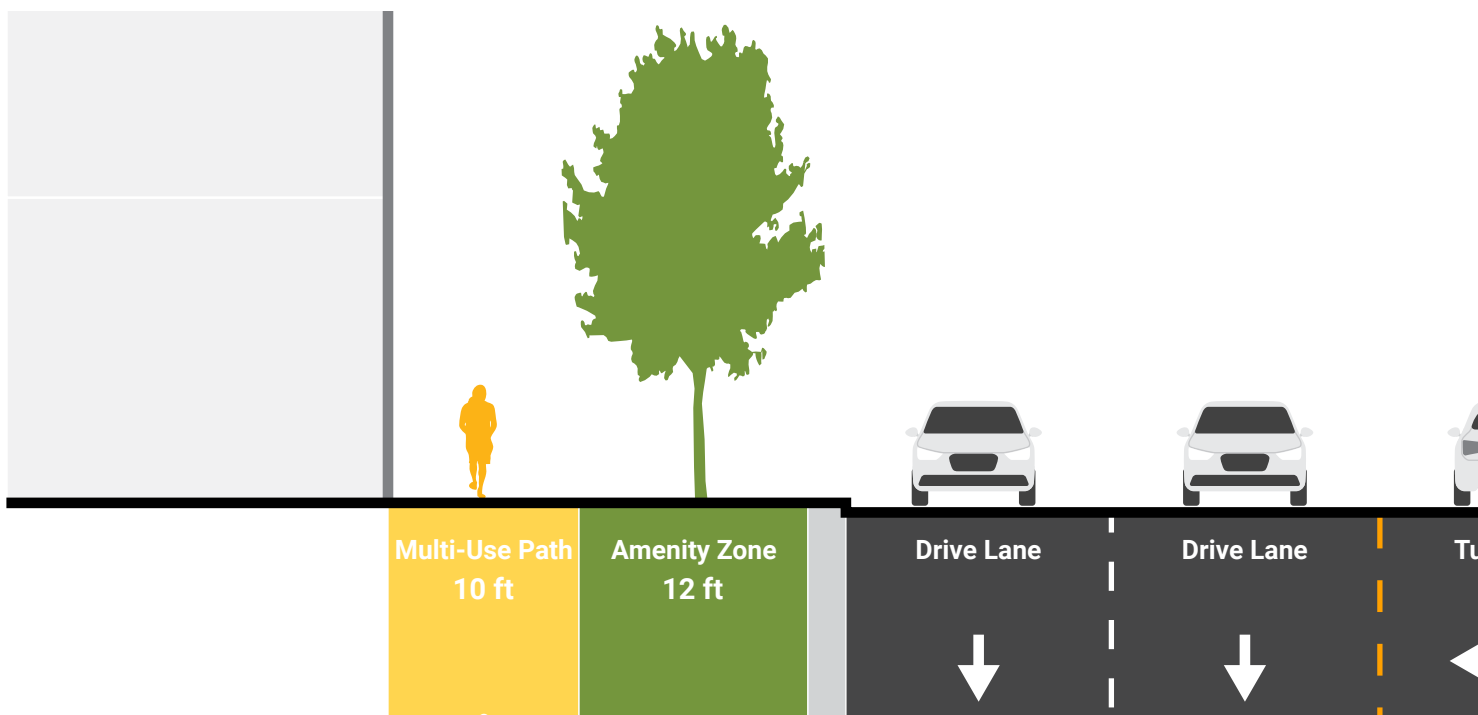
Note: 78th Avenue (drive lanes and turn lanes) by DEN.



COLLECTOR: GUN CLUB ROAD

INTENT

- » Distinguish Gun Club Road as a key gateway into West Approach District and a primary means of access into the District from Peña Boulevard.
- » Enhance multi-modal connectivity with a multi-use path.
- » Maximize the operational efficiency for airport traffic and prioritize passenger movements.

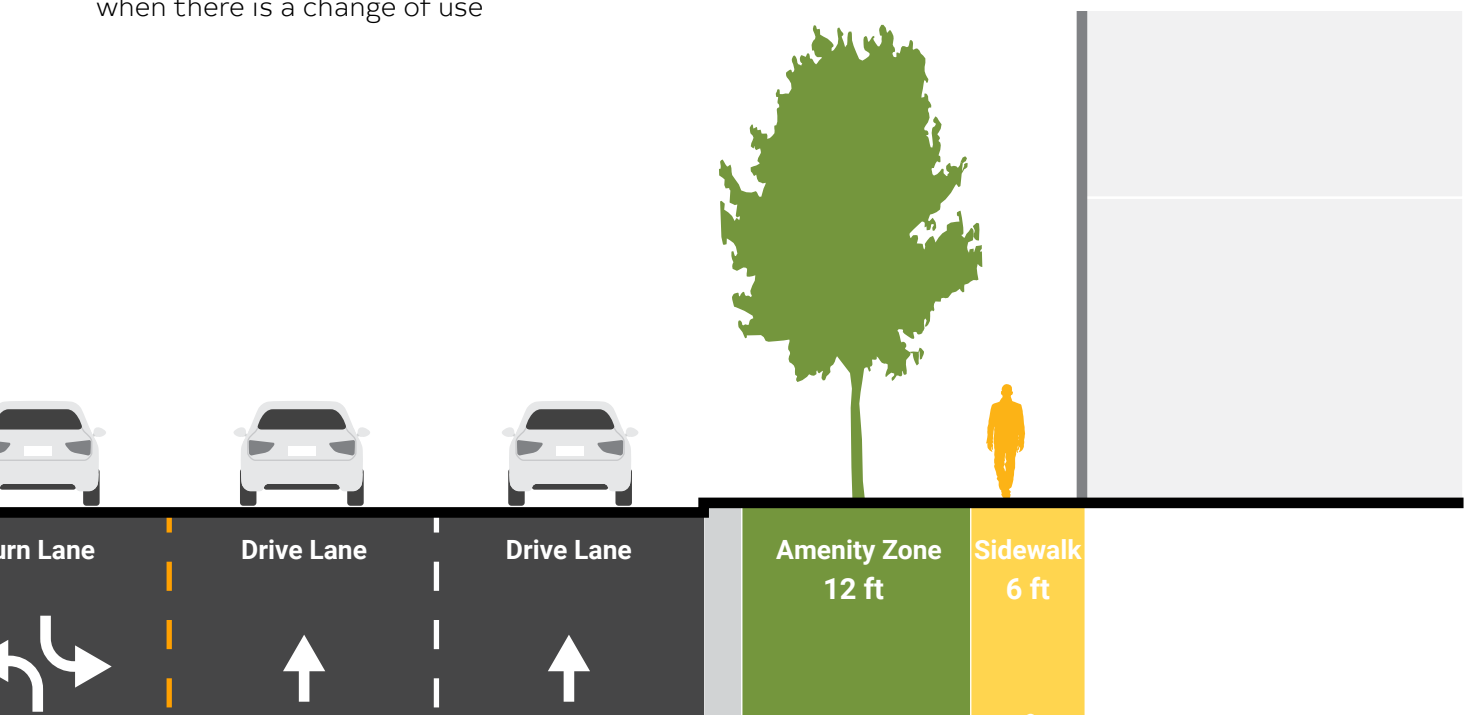


STANDARDS

- » Illustrative section — (see section below) shall be representative of Gun Club Road.
- » Curb cuts and vehicular access to parcels shall not be permitted from Gun Club Road.
- » The streetscape shall encompass a minimum of twenty-two feet (22') including a twelve feet (12') Amenity Zone and ten feet (10') Multi-Use Path.
 - A clear Multi-Use Path shall be organized to create a continuous unobstructed bike and pedestrian zone, a minimum of ten feet (10') wide, for ease of travel and maintenance.
 - An Amenity Zone shall be organized to create a continuous area, a minimum of twelve feet (12') wide and shall contain streetscape elements such as signage, benches, trash and recycling receptacles, and bike racks.
- » No on-street parking shall be provided.
- » Turn lanes and acceleration lanes shall be provided based on traffic needs.

Note: Gun Club Road (drive lanes and turn lanes) by DEN.

East side streetscape to be evaluated when there is a change of use

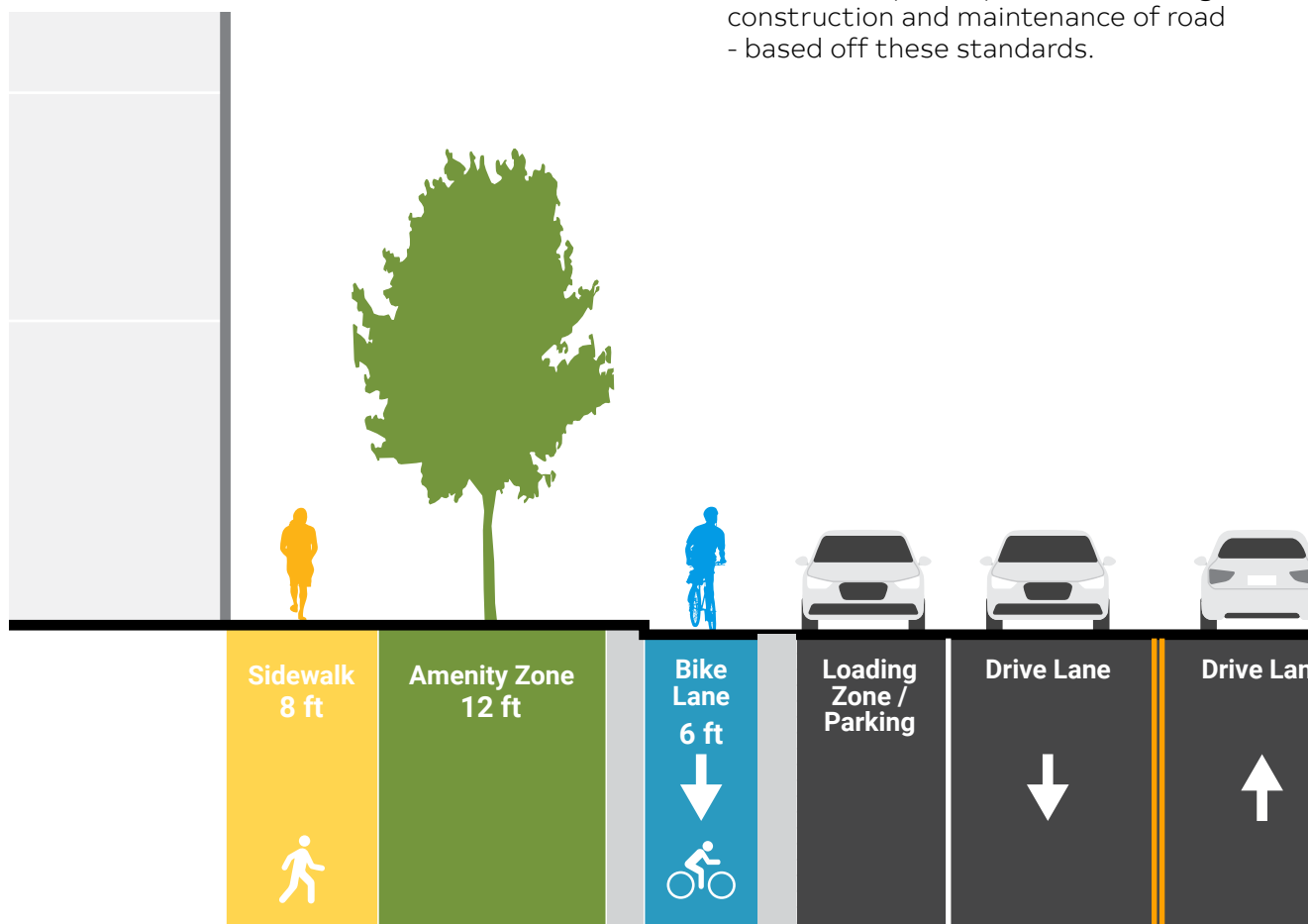


INTERIOR: 77TH AVENUE

INTENT

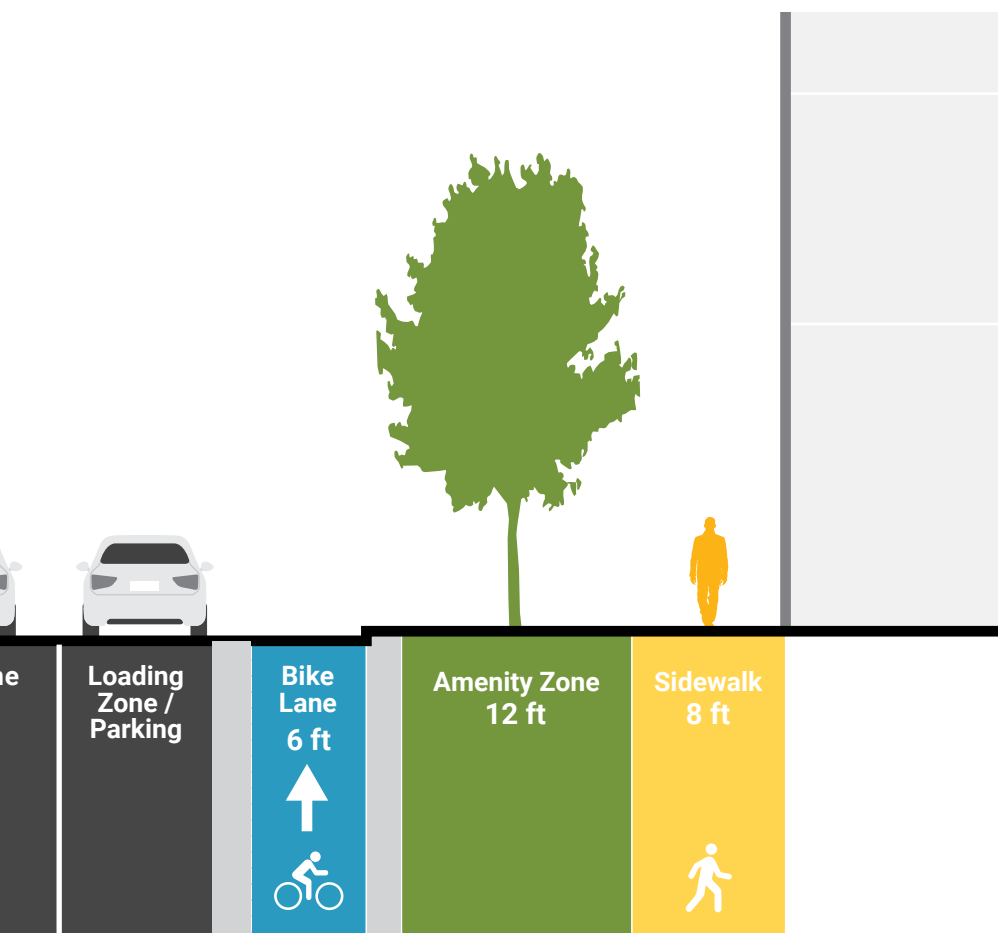
- » Retain the primary means of access to the center of the District from Gun Club Road.
- » Position 77th Ave to become a central multi-modal connection way to future development of Phase 2 on the west side of 78th Avenue.
- » Provide comfortable seating and gathering spaces for pedestrians that complement adjacent building uses and correspond with the rhythm of the streetscape design.

Note: Developer responsible for design, construction and maintenance of road - based off these standards.



STANDARDS

- » Illustrative section – 77th Avenue (see section below) shall be representative of the road.
- » Both sides of 77th Ave shall have a streetscape encompassing a minimum of twenty feet (20') including an Amenity Zone and Pedestrian Walking Zone.
 - A clear Pedestrian Walking Zone shall be organized to create a continuous unobstructed Pedestrian Walking Zone, a minimum of eight feet (8') wide, for ease of travel and maintenance.
 - An Amenity Zone shall be organized to create a continuous area, a minimum of twelve feet (12') wide and shall contain streetscape elements such as benches, trash and recycling receptacles, bike racks and lighting.
- » The road shall have eight feet (8') parking and a minimum of twenty-five feet (25') clear between parking.
- » A bike lane shall be provided in each direction with a 2-3' buffer between parking and the bike lane.
- » On-street parking shall be provided on both sides of the street and be located between the drive lane and bike lane to provide protection for bicyclists.



PARTIAL COLLECTOR: 76TH AVENUE

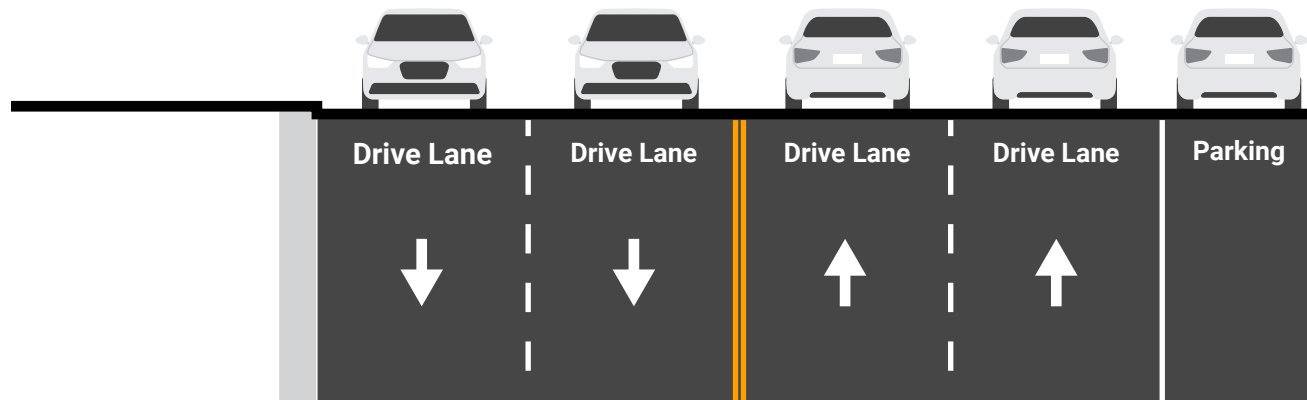
INTENT

- » Provide direct access to convenience and commercial-serving uses on the north side of Peña Boulevard.
- » Maximize visibility of vehicle-oriented uses to travelers on Peña Boulevard.
- » Provide a continuous streetscape that enhances the visual experience from Peña Boulevard.
- » Provide an efficient and easily accessible roadway with clear and direct routes to parking and services within the District.

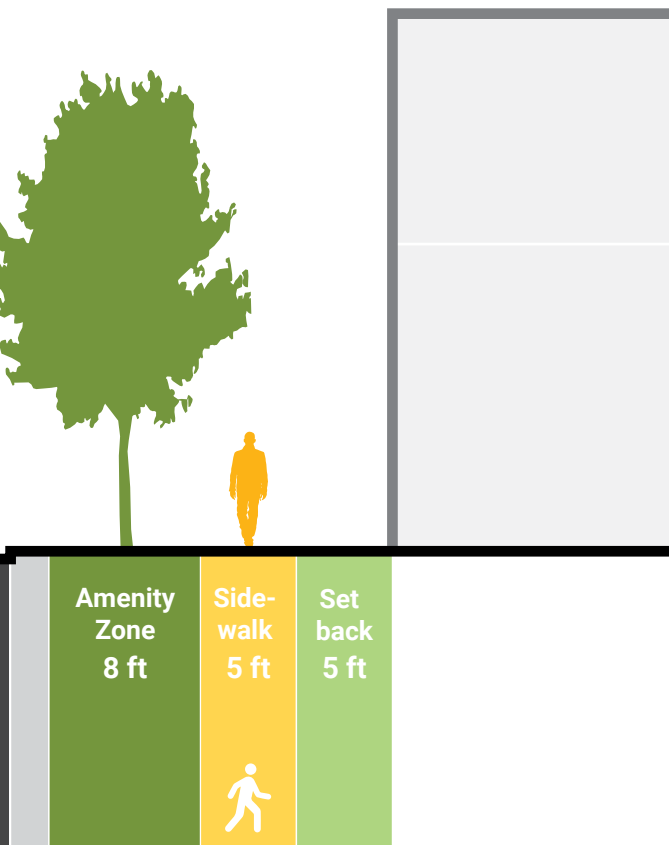
STANDARDS

- » Illustrative section — (see section below) shall be representative of 76th Avenue.
- » The north side of the road shall have a streetscape encompassing a minimum of thirteen feet (13') including an eight feet (8') Amenity Zone and five feet (5') Pedestrian Walking Zone.
 - A clear Pedestrian Walking Zone shall be organized to create a continuous unobstructed Pedestrian Walking Zone, a minimum of five feet (5') wide, for ease of travel and maintenance.
 - An Amenity Zone shall be organized to create a continuous area, a minimum of eight feet (8') wide and shall contain streetscape elements.

Note: The number of east-bound lanes will be determined during the West Approach Mobility Study. Section is representative of the maximum number of drive lanes.



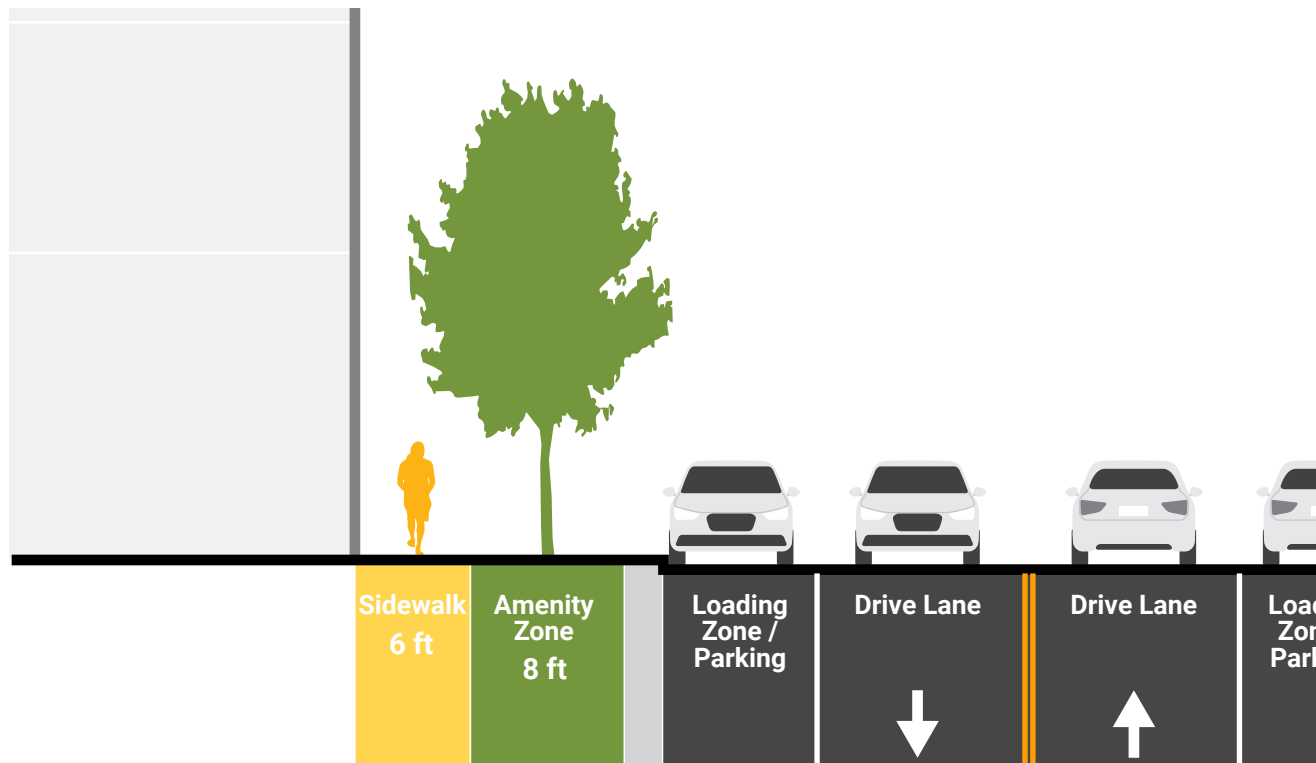
- » The road shall have eight feet (8') parking lane.
- » On-street parking shall only be provided on the north side of 76th Avenue, adjacent to development.
- » Curb cuts shall be minimized to the greatest extent possible.
- » Bus stop location shall utilize the parking lane.
- » Bus stop shelter shall be located next to the bus stop and within the amenity zone.
- » Sidewalk shall be unobstructed and clear behind the bus stop shelter.



INTERIOR STREETS

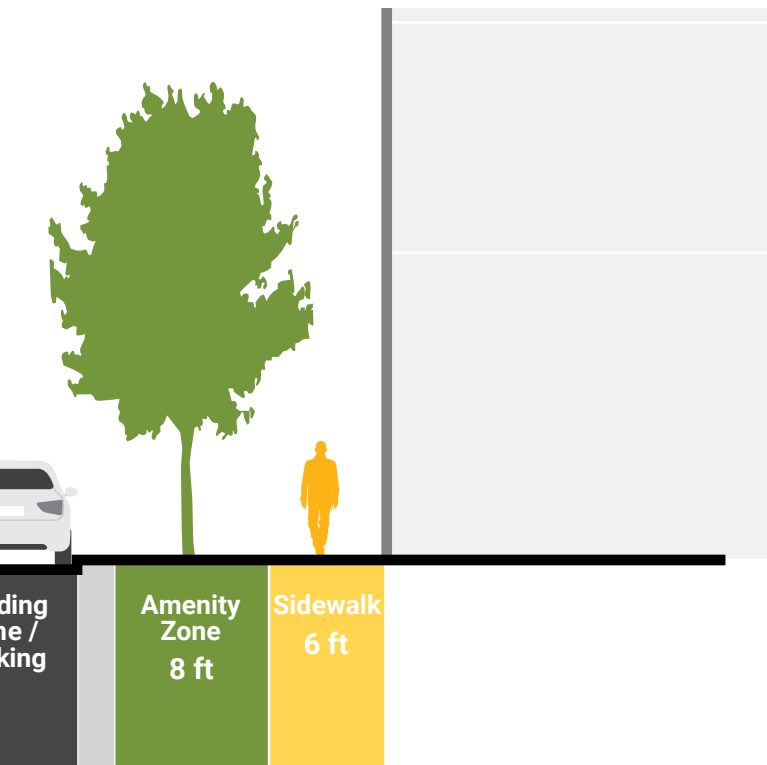
INTENT

- » Provide convenient and safe north-south connectivity within the District for vehicles and pedestrians.
- » Reinforce district character with a continuous streetscape.
- » Allow parcel access for vehicular, service and delivery traffic as identified in Chapter 02 District Layout.



STANDARDS

- » Illustrative section — (see section below) shall be representative of Interior Streets.
- » Both sides of an interior street shall have a streetscape encompassing a minimum of fourteen feet (14') wide including an Amenity Zone and Pedestrian Walking Zone.
 - A clear Pedestrian Walking Zone shall be organized to create a continuous unobstructed Pedestrian Walking Zone, a minimum of six feet (6') wide, for ease of travel and maintenance.
 - An Amenity Zone shall be organized to create a continuous area, a minimum of eight feet (8') wide and shall contain streetscape elements.
- » The road shall have eight feet (8') parking and a minimum of twenty-five feet (25') clear between parking.
- » On-street parking shall be provided on both sides of the street.
- » A maximum of 1 passenger loading zone shall be provided per block face.



FURNISHINGS

INTENT

- » Enhance the pedestrian experience and encourage pedestrian activity in the public realm, including streetscape and amenity zones.
- » Unify and reinforce the overall character and identity of the public realm throughout the District.
- » Encourage non-vehicular modes of transportation to and around the District.

STANDARDS

- » Street furnishing elements shall be high quality with consideration for long-term durability and maintenance and shall consist of complementary palette of benches, trash and recycling receptacles, bike racks and all other street and site furnishings.
- » Sidewalk benches shall be oriented so they will not impede pedestrian movement and shall not block the minimum unobstructed pedestrian walking zone.
- » Location and quantity of seating shall be designed to serve bus stops, building entrances and plazas or publicly accessible open space areas.
- » Separate trash and recycling receptacles shall be provided.
- » Safety, and comfort shall be primary considerations in the type, design and placement of street furniture.
- » Adequate quantities of street furniture shall be evaluated and used in all publicly accessible areas, including benches, bicycle racks, and trash and recycling receptacles.
 - Along interior streets, a minimum of 2 benches, 2 bike racks, 1 trash receptacle, and 1 recycling receptacle shall be provided per block.
 - Along 77th Avenue, a minimum of 2 seating clusters (with 2 seating elements at each cluster), 4 bike racks, 1 trash receptacle, and 1 recycling receptacle shall be provided per block.
- » Furnishings for sidewalk cafes or outdoor seating adjacent to buildings is encouraged.
- » Provide benches at primary corners.
- » Provide trash and recycling receptacles and bike racks at primary corners.
- » Bike racks shall be located near building entrances.
- » The same bench shall be used along all streetscapes.

LIGHTING

STREET LIGHTING

INTENT

- » Provide street lighting to enhance feelings of pedestrian security and safety.
- » Minimize glare and negative impacts on private properties.
- » Provide lighting that avoids light pollution and is energy efficient.

STANDARDS

- » Street lighting shall meet the requirements outlined in DEN's Electrical Design Standards Manual, Chapter 5 Lighting Systems and the City and County of Denver's Street Lighting Design Guidelines.
- » Light pollution off of the parcel boundary shall be reduced as much as possible, including light spillage from signage.
- » Surface parking lighting shall utilize cut-off type fixtures.
- » Light poles shall be uniformly spaced and aligned with street trees in the amenity zone.
- » Consideration shall be given to adjustments in street light placement to account for existing conditions or utilities while still maintaining a uniform spacing along the street.
- » Small cell infrastructure shall be incorporated into street lighting to the greatest extent possible to minimize number of poles within the streetscape.
- » Overall height of the light pole and fixture shall be subject to review and approval by the FAA.

PEDESTRIAN LIGHTING

INTENT

- » Reduce light pollution.
- » Increase pedestrian safety at night and overall site security.
- » Increase visibility of pedestrians to vehicular traffic at intersections, street crossings and building entrances.
- » Enhance aesthetic qualities of the streetscape and create a comfortable nighttime ambiance in the public realm.

STANDARDS

- » Fixture locations and illumination levels shall be designed to avoid extreme contrast between light and shadow.
- » Pedestrian lighting shall be an average of one (1) foot candle along internal pedestrian sidewalks and walkways and shall have a uniformity ratio of approximately 3 to 1 in publicly accessible open space and plazas.
- » Pedestrian lighting shall be provided near intersections and building entrances.
- » Pedestrian lights along all streetscapes shall consist of only one fixture type.
- » Pedestrian lighting fixtures shall be a maximum of eighteen feet (18') tall and be full cut-off.
- » Pedestrian lighting shall be spaced evenly and fixtures shall align with each other along the length of a street, pedestrian walkway or corridor.
- » The use of single luminaries is preferred over multiple luminaries.



ARCHITECTURAL PAVING

INTENT

- » Unify the public realm and streetscape with deliberate selection of paving types and special patterns or materials.
- » Clearly delineate pedestrian from vehicular areas through distinctive paving materials and patterns.

STANDARDS

- » Paving in the pedestrian walking zones shall be concrete or a special paving material or pattern.
- » Where paving is used in an amenity zone it shall be distinguished by the use of specialty patterns and materials to enhance the aesthetic.
- » Special paving material shall be used to enhance pedestrian areas associated with building entries and building related amenity areas.
- » The paving shall incorporate use of sustainable materials, processes or methods.
- » Pedestrian striping or enhanced pavement shall be provided at crosswalks.
- » All paving materials and patterns shall meet ADA requirements.



LANDSCAPE AND PLANT MATERIAL

INTENT

- » Select plant materials thoughtfully to create continuity between the surrounding open space and development areas and provide a number of benefits including improved air and water quality.
- » Respect the location and native environment of West Approach.
- » Provide high quality and well-maintained landscape throughout the project that enhances overall property image and creates a sense of project pride and identity.

STANDARDS

- » Native/adapted and low water use plant material shall be preferred. Plant diversity and the inclusion of pollinator friendly plant species is encouraged.
- » The use of high-water turf grass is discouraged except for high traffic areas. Any areas utilizing turf shall be sodded, not seeded.
- » Plants treated with neonicotinoids or other bee killing chemicals at any stage in their germination shall not be used.
- » Plant material shall conform to the following minimum size requirements unless otherwise specified in these guidelines:
 - Deciduous trees (2.5" caliper)
 - Ornamental trees (2" caliper)
 - Large evergreen trees (8' ht)
 - Small evergreen trees (6' ht)
 - Upright shrubs (4' ht)
 - Shrubs (5 gallon container)
 - Perennials (1 gallon container)
 - Grasses (1 gallon container)
 - Ground cover (4" pots)

- » Tree canopies shall not overlap and shall adhere to USDA guidance.
- » Enhance existing plant communities in the drainage ways with selections that are durable and low-maintenance.
- » Mulch shall be provided within all planting beds.
- » Approved plant lists are updated yearly by the USDA and shall be referenced for selection of plant material at DEN. All landscape materials shall be reviewed and approved by DEN and the USDA.
- » All landscape materials shall consider the surrounding landscape context and shall coordinate with DEN campus-wide landscape plans and guidance.



IRRIGATION

INTENT

- » Establish installation practices to ensure quality healthy landscape.
- » Utilize low water use irrigation systems, technologies and applications throughout the project to manage and conserve water use.

STANDARDS

- » All irrigation systems shall comply with Denver Water standards.
- » All landscaping shall be irrigated by an automatic underground irrigation system. If the plantings are xeric, the irrigation system to everything but trees may be turned off after establishment.
- » The use of spray irrigation shall be limited to turf areas only. All other areas shall minimize use of spray irrigation.
- » All irrigation systems shall install rainwater sensors.
- » Irrigation systems shall be maintained regularly to insure that the system is functioning properly and there is no ponding of water at any time.

GRADING & DRAINAGE

INTENT

- » Limit the impact of site grading and drainage improvements on the project site and adjacent areas, especially frontages along Peña Boulevard, Gun Club Road, and 78th Avenue.
- » Integrate the design of the grading and drainage into the overall site development.
- » Encourage the use of water quality and storm water management best practices, including innovative methods that create amenities.

STANDARDS

- » If site grading and drainage improvements encroach into the landscape buffer, they shall be designed as a site amenity.
- » Retaining walls shall be limited to 30" maximum height and shall incorporate materials in keeping with the architectural style of their associated building and overall character of West Approach.
- » Rails are required on retaining walls over 30" that are adjacent to pedestrian access. All rails shall be in keeping with the architectural style of the associated building and character of West Approach.
- » Primary site drainage will be accommodated for on a regional basis.
- » Snow storage shall be accommodated for in a manner that does not interfere with pedestrian or vehicular traffic, nor in a location that damages existing vegetation.
- » All water quality and detention areas shall be designed and landscaped to be visually pleasing through the use of plantings, aesthetic treatments to drainage infrastructure, and character consistent with the overall district.
- » Site grading shall generally avoid slopes exceeding 10:1 to allow the available open space to be usable.
- » Maximum grades for landscape areas shall not exceed 4:1.
- » Water quality and detention shall be handled with bio-swales, street side stormwater planers, bio-retention tree trenches, permeable pavers in alleyways or other best practices in coordination with DEN's division of Airport Infrastructure Management, to the greatest extent possible.

SIGNAGE

SIGNAGE

84 DISTRICT SIGNAGE

88 BUILDING SIGNAGE

88 UNIVERSAL STANDARDS

94 TENANT IDENTIFICATION SIGNAGE

Signage is intended to express the unique identity of West Approach by reflecting both sustainability and technology. Overall, signage within West Approach shall create a cohesive aesthetic that brings together the building signage to other signage elements throughout the District. The purpose of signage within the District is to help visitors navigate to and through the District as well as inform users of the uses within the District. The signage shall be designed to have a common look and theme creating cohesiveness across the District.



DISTRICT SIGNAGE

INTENT

- » Reflect the unique character of West Approach, reinforce DEN's identity as a global leader in aviation, and its position in the high plains prairie at the foot of the Rocky Mountains.
- » Facilitate navigation into and through the district.
- » Ensure a consistent appearance and cohesive aesthetic on all District signage.
- » Establish a hierarchy between signage elements throughout the District.
- » Promote design that maximizes graphic impact rather than square footage.
- » Integrate signage in a manner that facilitates commerce, enlivens the public realm, and enhances and respects the character of the District.
- » Ensure that the signage does not detract from airport related wayfinding and signage.
- » Acknowledge the primary role of the Denver International Airport Welcome sign within Peña Boulevard and position District signage as secondary to this.

STANDARDS

General Standards

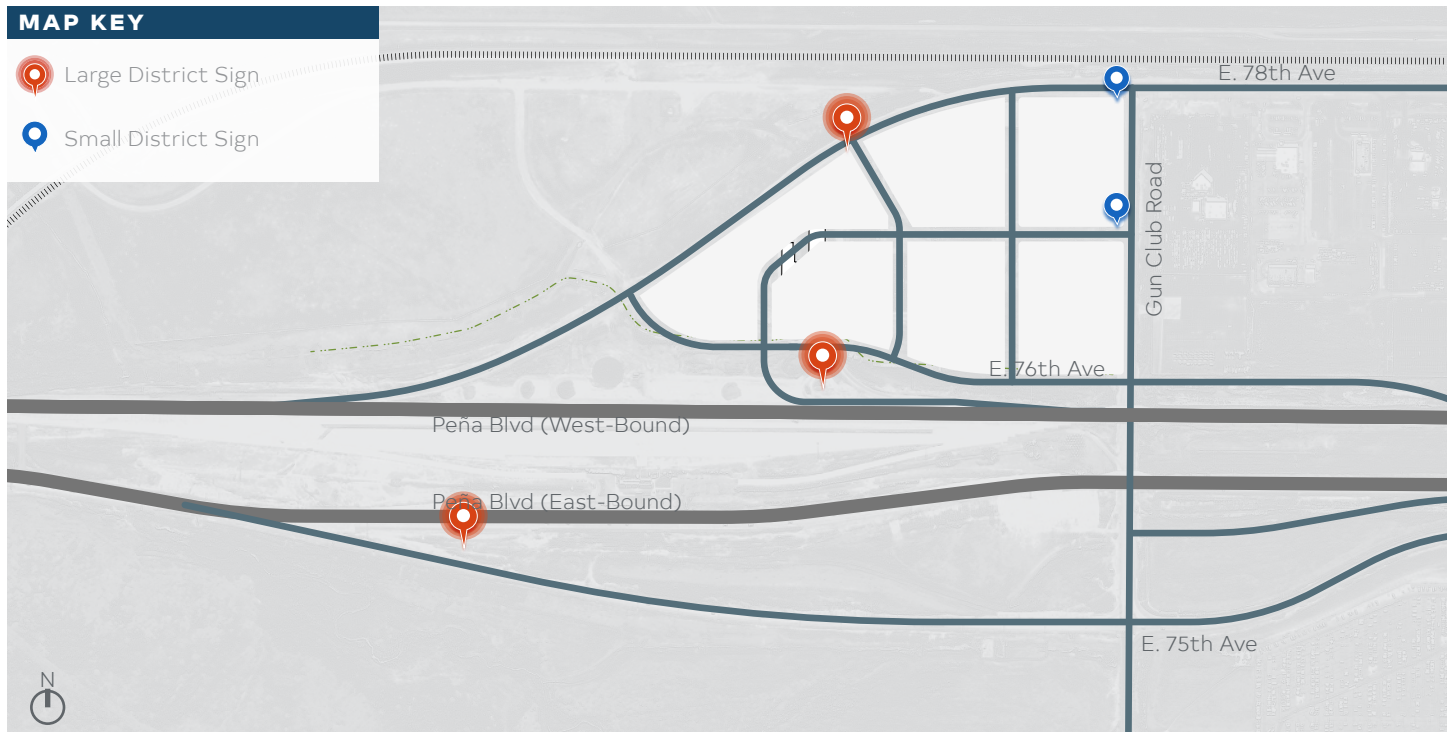
- » District signage shall not block the view to any airport related signage.
- » Signage shall be smaller and secondary to the Denver International Airport Welcome sign.

Denver International Airport Welcome Sign



- » Lighting within signs shall not be directed upward. All sign illumination and light features are subject to FAA review and approval.
- » All exterior District signage shall be designed with a common palate of materials and consistent theme that is coordinated with DEN.
- » All materials used in signage shall be durable, of permanent nature, require minimum maintenance, and be resistant to weathering and staining.
- » Locate signs away from edge of roadway and select appropriate base materials to minimize impacts of chemicals to melt snow and ice.
- » Signage shall have no exposed wiring, conduits, tubing, lamps, ballast boxes, or raceways. All cabinets, transformers, ballasts, attachment devices, and similar equipment shall be concealed.
- » No private signage shall be posted or attached to trees, public utility improvements, lighting poles or fixtures, traffic signage, or traffic control devices.
- » No private signage shall be posted, placed or erected within streets outside parcels, unless approved by DEN.
- » Location of signage shall be considered to avoid "visual noise" along the roadways.

Signage map



Large District Signage

- » The design and character of all signs shall be the same to promote a cohesive district-wide character.
- » Signage shall fit within and overall area of fifteen feet (15') tall by twenty feet (20') wide by ten feet (10') deep. The height is measured from the existing grade. Berming is not permitted to increase the height of the sign.
- » Dynamic signage shall be permitted.
- » Signage shall be integrated into the surrounding landscape where signage is placed.
- » Signage materials shall be consistent with the overall district character.
- » Up to three (3) district signs are allowed. Signage shall be located in highly visible areas to announce the District to approaching or passing visitors in the general locations identified on the signage map.
- » Advertising is not permitted on signage.
- » Large district signage is for district identification and shall only include the name of the district. Non-DEN logos or additional text shall not be allowed.

Small District Signage

- » Signage shall be located in highly visible areas and at the intersection of two roads.
- » Signage shall be located behind sidewalk zone.
- » Signage shall not impede upon the site triangles at intersections.
- » Advertising is not permitted on signage.
- » Signage shall only include district identification.
- » Signage shall be consistent to promote a cohesive district-wide character.
- » Signage shall fit within and overall area of ten feet (10') tall by twelve feet (12') wide by five feet (5') deep.
- » Dynamic signage shall not be permitted.
- » Signage shall be smaller but similar in character to the Large District Signage.



BUILDING SIGNAGE

UNIVERSAL STANDARDS

SIGN TYPES AND QUANTITY

INTENT

» To ensure consistency of type and location and quality to create a cohesive public realm.

STANDARDS

- » Sign Types are permitted as described in Table 8.1, according to the appropriate tenant/building type. They shall be designed according to their specific standards.
- » Temporary signage, including banners, are not permitted.
- » Each tenant is permitted a maximum of six (6) signage, or three (3) signage per frontage on a street or open space, whichever is greater. Awning Signage and Window signage, which are typically repeated on multiple awning or window panels, shall only count as one sign.
- » The total sign area permitted for each building/tenant type is listed in Table 8.2. No sign shall exceed 200 square feet, and the total sign area of any tenant shall not exceed 600 square feet with the exception of hotel / hospitality which requires sign approval.
- » All tenant signage must be contained within the tenant's lease frontage.
- » Signage shall be subordinate to and compatible with the overall design of the building.
- » Signage shall be oriented and illuminated so that they do not adversely affect existing nearby uses.
- » Animated and digital signs are permitted.

DEN reserves the right to reject any signage proposal it deems to be inferior design, quality, or durability. DEN also reserves the right to request the removal of any advertising or promotional signage it deems inappropriate.

Alternatively, this signage shall be removed by the DEN at the building owner's expense.

TABLE 8.1 PERMITTED SIGN TYPES

TENANT / BUILDING TYPES	SIGN TYPES				
	Major Tenant identification	Storefront Identification	Project Blade Sign	Awning Sign	Window Sign
77th Ave Anchor		✓	✓	✓	✓
Major Building Tenant	✓	✓	✓	✓	✓
In-line Tenant Storefront		✓	✓	✓	✓
Hotel / Hospitality	✓	✓	✓	✓	✓



APPEARANCE AND DESIGN

INTENT

» To ensure that the appearance of all signage is consistent with its surrounding architecture and the vision for West Approach.

STANDARDS

- » All exterior signage shall address both the communicative functions of a sign and its aesthetic integration with the overall building concept.
- » All signage shall be limited to trade names or company logos.
- » All tenant signage shall be appropriate to and expressive of the tenant business activity for which they communicate.
- » Tenant signage and logos shall express a refined urban sophistication using clean and contemporary shapes and forms. The tenant is encouraged to use color, typography, pattern, texture and materials to create a dynamic interface with the streetscape.
- » Tenant sign design shall comply with all architectural design standards and be compatible with and an enhancement of the architectural character of the surrounding district, expressing scale, color, materials, and lighting levels. Sustainable materials, fabrication and installation methods are highly recommended.



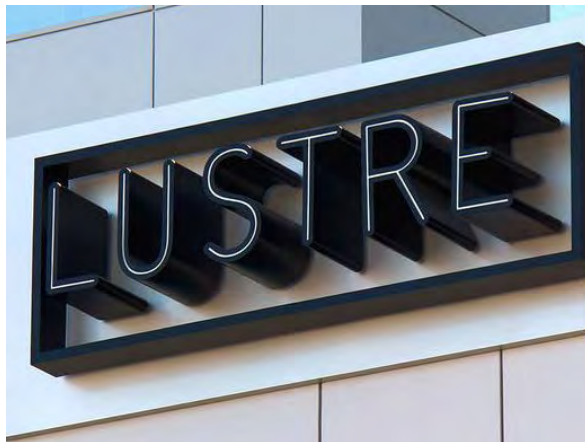
TABLE 8.2 PERMITTED SIGN TYPES

TENANT / BUILDING TYPES	TOTAL PERMITTED SIGN AREA FOR ANY TENANT (SQ. FT.)	MAXIMUM SINGLE SIGN AREA	CALCULATION OF TOTAL PERMITTED SIGN AREA
77th Ave Anchor	600 max.	By sign type	2 square feet of sign area for each linear foot of building frontage along 77th Avenue, plus 1 square feet of sign area for each linear foot of frontage along another street or open space.
Major Building Tenant	400 max.	By sign type	2 square feet of sign area for each linear foot of building frontage along a street for the first 150 feet of building frontage, then 1 square feet of sign area for each linear foot of frontage thereafter.
In-line Tenant Storefront (ground floor tenant in a multi-tenant building)	80 max.	By sign type	1.5 square feet of sign area for each linear foot of building frontage along a street for the first 50 feet of building frontage, then .5 square feet of sign area for each linear foot of frontage thereafter.
In-line Tenant Storefront (less than 25 feet of frontage)	40 max.	By sign type	NA
Hotel / Hospitality	Requires size approval	Requires size approval	Limited to 2 signs on the upper part of the building, one sign at the entrance, one awning or blade sign, and one monument sign, all requiring size approval.

MATERIALS AND COLORS

STANDARDS

- » Materials used in exterior applications shall be rated for exterior use.
- » The following materials and fabrication methods are not permitted:
 - Cast letters in plaster of paris or papier mache, plexiglass or any cardboard fabrication.
 - Any fabrication with exposed fasteners unless architecturally integral to the building or signage design.
 - Styrofoam, rubber, or carpet fabrication.
 - There shall be no exposed conduit, tubing, raceways, conductors, transformers, or related equipment that is not visually integrated with the overall sign appearance.
 - Cabinet sign boxes are discouraged unless they conform to the shape of the tenant logo/logotype.
 - Visible sign fabricator's stickers.
 - Hand painted letters, logos, and or graphics on any signage or storefront glazing, unless they are integral to the company logo.
 - Continuous neon banding elements.
 - Day-Glo or reflective paints are not permitted.
- » All connecting hardware shall not be visible unless it is an integral part of the sign design.



SIGNAGE LIGHTING

STANDARDS

- » Custom armature with integrated light fixtures shall be integrated into canopy design.
- » Exposed neon with-in reverse channel type letters or logos are permitted.
- » Wattage for signage must be noted on fabrication drawings for review and approval.
- » A dimmer switch shall be located inside the sign cabinet or other accessible location near the sign and allow for adjustments in the illumination of such signage.
- » Spot lighting from an opposing structure is not permitted.
- » Illumination shall not flash, blink, fluctuate or be animated.
- » Signage with both architectural and integrated lighting are encouraged to use a LED light source.



TENANT IDENTIFICATION SIGNAGE

MAJOR TENANT IDENTIFICATION

INTENT

- » To identify the major tenant within a single or multi-tenant building.

STANDARDS

Eligibility

- » Major Tenant Identification signage are permitted for hotel retail, commercial and/or entertainment tenants over 5,000 square feet in single or multi-tenant buildings, as well as identifying primary entries and lobbies.

Type and Quantity

- » Major tenant identification signage shall be wall mounted signage only.
- » A maximum of two (2) signage are permitted per building.
- » A maximum of one (1) sign is permitted per building elevation.

Size and Dimensions

- » Signage shall not exceed the following area:
 - 120 square feet maximum area per sign.
 - 200 square feet total for all Major Tenant Identification signage.
- » The total sign width shall not exceed 2/3 of the tenant's facade or other significant building element to which it is attached.
- » Where signage is integrated into a larger artistic expression or lighting element, the size standards shall only apply to the text area.
- » The total projection from the wall or fascia shall not exceed ten inches (10").
- » Heights of letter and/or logos shall not exceed two-feet, six-inches (2'-6").

Location

- » Signage may be placed above the ground floor uses.
- » Signage shall not overlap or cover features of the building, such as cornices, eaves, windows, door frames, columns and other decorative elements.
- » Signage shall not protrude above the roof line.

IN-LINE TENANT STOREFRONT IDENTIFICATION

INTENT

» To identify a ground-floor retail tenant within a multi-tenant building or within a freestanding building under 5,000 square feet.

STANDARDS

Type and Quantity

- » Tenant Identification signage shall be wall mounted signage only.
- » A maximum of one (1) sign is permitted per building frontage on a street or open space.

Size and Dimensions

- » The area of Tenant Storefront Identification signage 77th Avenue shall not exceed sixty (60) square feet. Signage facing all other streets shall not exceed twenty-five (25) square feet.
- » The total sign width shall not exceed 2/3 of the tenant's store- front or other significant building element to which it is attached.
- » Heights of letter and/or logos shall not exceed two-feet, six-inches (2'-6").
- » The total projection from the wall or fascia shall not exceed ten inches (10").

Location

- » Signage shall be integrated with the building facades or above main entries of tenant lease spaces.
- » Tenant storefront signage shall be mounted directly to the wall or parapet wall of the building between the top of the storefront and the second level finished floor.

TENANT PROJECTING BLADE SIGNAGE

INTENT

» To identify a ground-floor retail tenant within a multi-tenant building or within a freestanding building under 5,000 square feet.

STANDARDS

Quantity and Size

- » Each tenant may install one (1) projecting blade sign for each separate entry, or each frontage facing a street or open space.
- » The total sign volume shall not exceed thirty (30) cubic feet or a maximum height of four feet (4').
- » Signage shall not project more than four feet-six inches (4'- 6") from the building face.
- » The minimum clear distance from any pedestrian way to the bottom of a projecting sign shall be eight feet (8').

Location

- » All projecting signage shall be two-sided, mounted perpendicular to the facade.
- » Signage shall be mounted directly to the wall or parapet wall of the building with perpendicular blade armature.
- » Signage shall not extend above the sill line of the second story.
- » Near project awnings or entry canopies, the mounting height of projecting signage shall be set to avoid their being obscured by the awning or canopy.

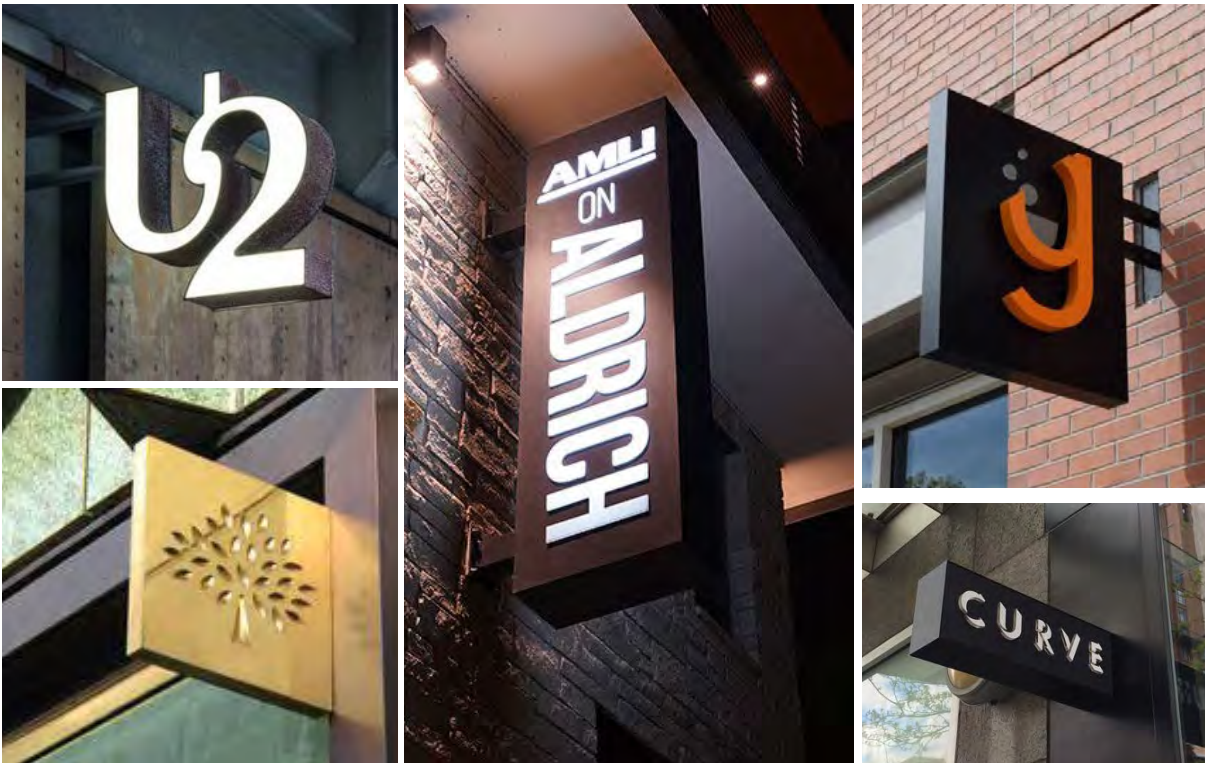
Fabrication

The following approaches are permitted:

- » Iconographic or sculptural elements.
- » Metal frame/armature to reflect approved architectural material and color palettes.
- » Architectural metals and painted metals recommended.
- » Tenant logo/logo type to be fabricated or flat cut-out and layered to achieve a three-dimensional form to the signage components.
- » Sign armature consistent with building design standards.

Illumination

- » Signage can have either external or internal illumination. Exterior lighting shall be integrated into armature.



AWNING AND CANOPY SIGNAGE

INTENT

- » To identify a ground-floor retail tenant within multi-tenant building.

STANDARDS

Quantity and Size

- » One (1) sign is permitted per awning. Tenants may include their store name or logo on the primary sloped panels and/or the vertical valence panels of awnings.
- » The maximum area of graphics, logos and/or text shall not exceed ten (10) square feet.

Fabrication

- » Graphics, logos and/or text shall be silkscreen painted or sewn onto the awning fabric.

Illumination

- » Backlit or internally illuminated awnings are not permitted.
- » External lighting of awning with par lamps or other unobtrusive light fixtures is acceptable. External lighting shall be integrated into armature.



ENTITLEMENT AND APPROVALS

Entitlement to be guided by Commercial Districts at DEN: Development Approval Process flow chart, conditions defined in tenant lease agreement and DEN project manager.



TECHNICAL GUIDING DOCUMENTS

West Approach Campus is zoned DIA under Section 9.5 of the Denver Zoning Code, and there are no viewplane ordinances on the site. DEN will require that all developers and tenants development plans conform to the design standards described within this document.

All development in the West Approach District will also have to adhere to recommendations made by the United States Department of Agriculture (USDA) Airport Wildlife Hazards Program. This program provides guidance intended to reduce the likelihood of aviation-related wildlife strikes that threaten human health and safety. More information is provided at:

https://www.aphis.usda.gov/aphis/ourfocus/wildlifedamage/programs/SA_Airport/CT_Airport_hazards

In addition, the Federal Aviation Administration (FAA) provides additional information regarding Hazardous Wildlife Attractants, and how development should avoid introducing these elements, in AC 150/5200-33 (February 21, 2020).

Additional DEN documents referenced in these standards or which may otherwise be relevant include the following:

- » **DEN Real Estate Strategic Development Plan (2018)**
- » **DEN Industrial Stormwater Management Plan (2020)**
- » **DEN Sustainability Management Plan (2013)**
- » **DEN Architecture Design Standards Manual (2021)**
- » **DEN Civil Design Standards Manual (2021)**
- » **DEN Sustainability Design Standards Manual (2021)**



